

	EXECUTIVE SUMMARY 04-05
01	INTRODUCTION .06-07 01.1 LOCATION .08 01.2 BACKGROUND .09-13 01.3 GOALS AND OBJECTIVES .14 01.4 WHAT IS A SIDE PATH? .15
02	EXISTING CONDITIONS 16-17 02.1 EXISTING INFRASTRUCTURE 18-19 02.2 FUTURE CONNECTIVITY 20-21 02.3 LAND USE AND ZONING 22 02.4 METROPOLITAN RIVER PROTECTION ACT 23 02.5 ECOLOGICAL REPORT 24 02.6 CULTURAL RESOURCES REPORT 25-26
03	TECHNICAL ANALYSIS

	DESIGN RECOMMENDATIONS	22.22
	04.1 PHASING PLAN	
	04.2 SITE AMENITIES	
	04.3 EASEMENTS	
	04.4 PHASE 1 PLANS	
	04.5 PHASE 2 PLANS	
	04.6 PERSPECTIVES	
	04.7 STREAM CROSSING ALTERNATIVES COMPARISON	
	04.8 MID-BLOCK CROSSING SIGHT DISTANCE STUDY	
	V4.0 IIIID-DEGOR GROOGING GIGHT DIGTARGE GTODT	
	COST ESTIMATE	86-87
	05.1 OVERALL ESTIMATE	
	05.2 FUNDING OPPORTUNITIES	90
06	PROJECT DURATION	
07	APPENDIX	96-97 98-109
	07.2 ECOLOGICAL REPORT AND CULTURAL RESOURCES REPORT	
	07.3 COORDINATION MEETINGS	134
	07.4 COST ESTIMATE	
	07.5 PHASE 2 ALTERNATIVES COMPARISON	141
	07.6 PHASE 2 ALTERNATIVE DESIGN	142-147
	07.7 PHASE 2 (SHEET 2.5) OPTION 2	
	07.8 RESOURCES	

EXECUTIVE SUMMARY

OVERVIEW

The proposed project would construct an approximate 2.2-mile shared use side path alongside Roberts Drive near Sandy Springs in Fulton County, Georgia. The project is planned for two phases between the intersection of Roswell Road and Dunwoody Place.

In addition to the side path project, the Georgia Department of Transportation (GDOT) SR 400 Express Lanes Project has slated two bridge replacements along Roberts Drive. These segments will be constructed by GDOT incorporating a multi-use side path with raised landscape buffer within the required right-of-way width on the new bridge construction.

The goal of this project is to create a safer and more comfortable walking and bicycling environment along Roberts Drive, and to increase connectivity to parks and greenspace within the City of Sandy Springs.

This scoping study was conducted to examine the possible ways of developing the Roberts Drive side path effectively and economically.



Georgia DOT SR 400
Express Lane Bridge
Typical Rendering
AtkinsRéalis



Morgan Falls Overlook Park Visit Sandy Springs



Abernathy Greenway Park North Visit Sandy Springs

METHOD

The design process involved in creating this scoping document included analysis of existing conditions, review of previous city planning documents, Georgia Department of Transportation (GDOT) SR 400 Express Lane project schedules and coordination with the City of Sandy Springs staff. AtkinsRéalis quickly proceeded with archeological and environmental research for the area impacted in the side path limits of work. Existing conditions on-site were examined to determine the alignment and design of the proposed shared use side path. Coordination with city staff and GDOT for the timeline was necessary for scheduling this project to coincide with the GDOT bridge replacement project. The project will be completed in two phases, the first phase located through industrial and commercial areas coincides with the first bridge replacement. The second phase will finish the side path and connect to Roswell Road.

This project meets the goals set forth in the comprehensive plans adopted by the City of Sandy Springs from years 2014 to 2022 as identified in section 01.2 Background.



Island Ford Chattahoochee River National Recreation Area Visit Sandy Springs



Sandy Springs Performing Arts Center Visit Sandy Springs

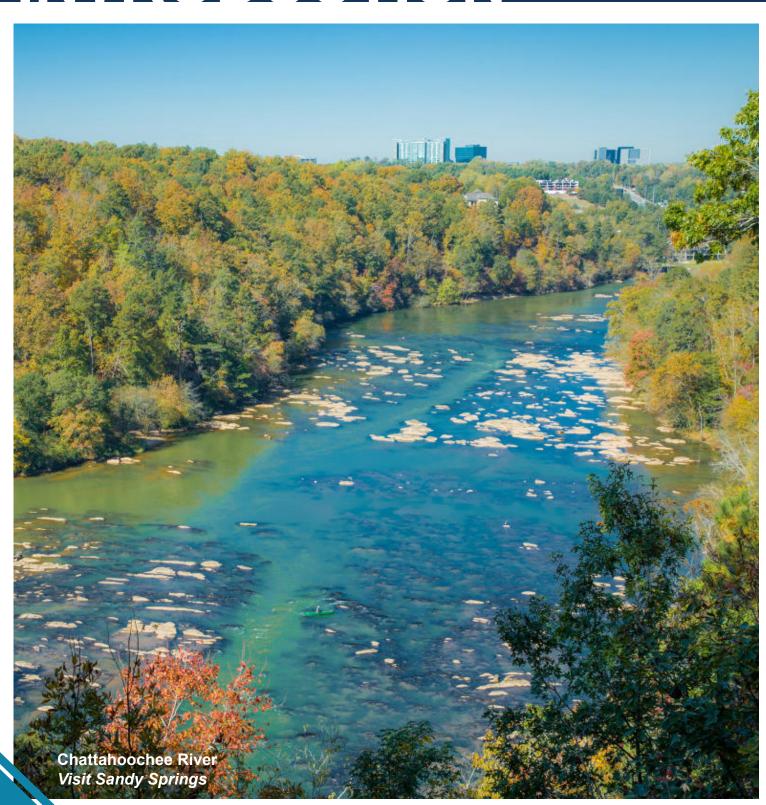


Interstate N Pkwy Side Path AtkinsRéalis



City Green at City Springs Visit Sandy Springs





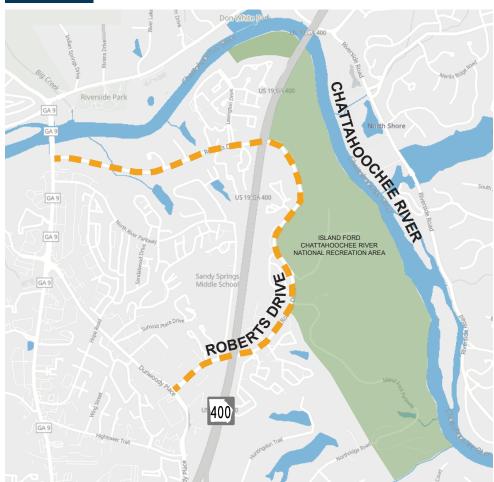
01.1 LOCATION

01.2 BACKGROUND

01.3 GOALS AND OBJECTIVES

01.4 WHAT IS A SIDE PATH?

01.1 LOCATION

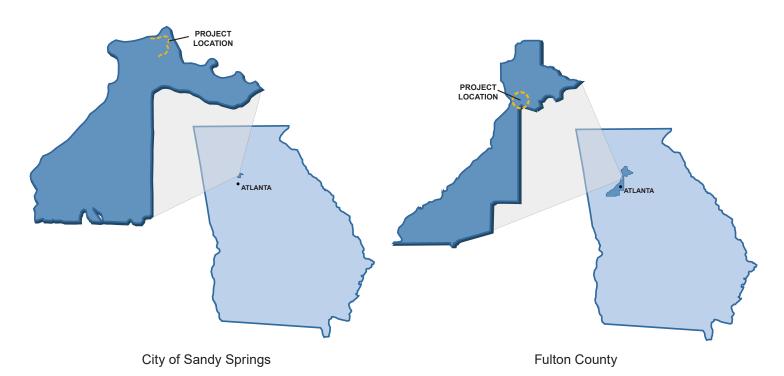


Map of proposed side path along Roberts Drive

80

The City of Sandy Springs is a 38.53 square mile city located about 12 miles north of Downtown Atlanta in northern Fulton County, Georgia, and is an inner ring suburb of Atlanta. The city's population was 108,080 at the 2020 census, making it Georgia's seventh-most populous city. It is the site of several corporate headquarters, including UPS, Newell Brands, Inspire Brands, Focus Brands, Cox Enterprises, and Mercedes-Benz USA's corporate offices.

The Roberts Drive side path project is in the northeastern part of Fulton County incorporating approximately 2.2 miles from Roswell Road to Dunwoody Place with the connection to Island Ford, the headquarters for the Chattahoochee National Recreation Area. This unit is located entirely within Sandy Springs.



01.2 BACKGROUND

The following studies were reviewed to become familiar with previous work conducted in Sandy Springs that would align the Roberts Drive side path scoping report with citywide goals:

Preliminary Alternatives Assessment, Improving Bicycle and Pedestrian Connectivity to the Island Ford Unit of the Chattahoochee River National Recreation Area (Hagen Thames Hammons,

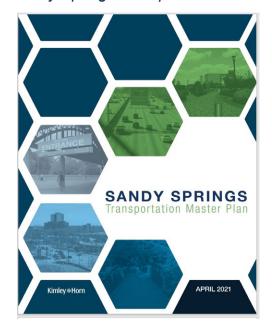
2015-16, NPF Transportation Scholar)



This report was completed to develop safer bicycle and pedestrian transportation alternatives to provide pedestrian links to the Chattahoochee National Recreational Area (CRNRA) at Island Ford Park. The goals are to decrease dependence on the automobile to access the unit, connect to the regional trail network and expand use of the CRNRA facilities to visitors and residents alike. The objective and outcome follow closely with an Environmental Assessment in accordance with the National Environmental Policy Act which evaluates the impacts of proposed project alternatives on the natural environment, historic resources, and public use and experience. Four action alternatives were developed and evaluated for discussion. This Scoping Report includes preliminary plans and technical data for approved Alternative 1 as outlined in this report as selected as the best option for Roberts Drive.

Alternative 1: A Multi-Use Side Path (MUSP)
parallel to a major collector or road that directly
connects the CRNRA to the future bridge.

Sandy Springs Transportation Master Plan, 2021



The Transportation Master Plan (TMP) completed by Kimley Horn (2021) for the City of Sandy Springs includes public input and connectivity data for improving roadways, trails, sidewalks, intersections, and transit facilities for the area. The TMP identifies Roberts Drive as priority for a bicycle, pedestrian, and trail connection.

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS

01.2 BACKGROUND

City of Sandy Springs Next Ten Comprehensive Plan 5-yr Update, adopted 2017, updated 2022



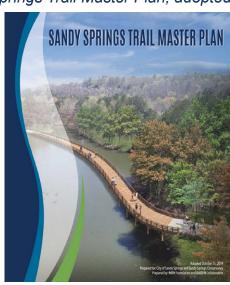
The comprehensive plan ranks developing an expanded trail network 9 out of the 10 action items. The plan denotes a priority of connecting green spaces, recreational facilities, destinations, and population centers within Sandy Springs with a network of interconnected on-road and off-road pedestrian and bicycle trails. Roberts Drive is identified in proposed on-road & road-adjacent bike and pedestrian connections.

Sandy Springs Recreation and Parks System Comprehensive Plan, February 2019



This report lists hiking trails and multi-use trails in the top five facility priorities needs for Sandy Springs Recreation and Parks System.

Sandy Springs Trail Master Plan, adopted 2019



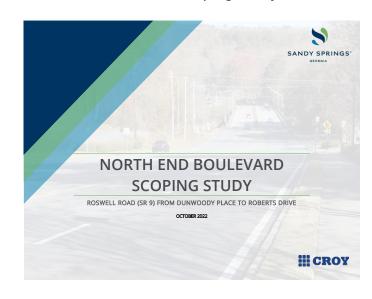
The top five priorities in this Master Plan lists multi-use trails as number two priority and identifies the Roberts Drive side path in the overall bicycle and pedestrian network plan. The plan recommends a 5-foot minimum landscaped buffer from the roadway should be required along with markings on the trail to heighten awareness that bicycles, and other users are present.

City of Sandy Springs TSPLOST, 2021



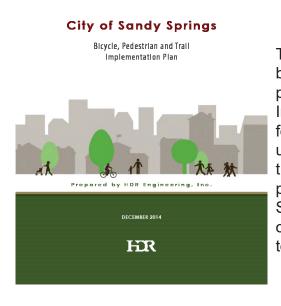
The 2021 Transportation Special Purpose Local-Option Sales Tax lists the Roberts Drive side path as a tier two project. TSPLOST tier 2 projects will only proceed if TSPLOST revenues exceed revenue projections.

North End Boulevard Scoping Study, October 2022



This study focuses on the Roswell Road corridor from Dunwoody Place to Roberts Drive, and lays out a recommendation for operational, safety, and multimodal improvements.

City of Sandy Springs Bicycle, Pedestrian, and Trail Implementation Plan, December 2014



This plan identifies the Roberts Drive side path in the overall bicycle and pedestrian network plan. Roberts Drive side path project aligns with Policy T-B2 (Bicycle and Pedestrian Improvements). Another key action in the comprehensive plan for connections from neighborhoods and multi-family apartment units to natural recreation areas to mitigate traffic congestion through a range of transportation options and linkages. This project meets the Green Space, Natural Systems, and Sustainability policies GS-C1 and C2 vision to enhance on-road links to local and regional parks and connect to green space network.

11

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS

01.2 BACKGROUND

SR 400 Express Lanes Bridge Replacement Project, 2023-Ongoing

The City of Sandy Springs prepared the SR 400 Managed Lanes Bridge Enhancements Aesthetic Design Guide (Dec. 2020) for three bridge projects within the city: Spalding Drive, Pitts Road, and Roberts Drive.

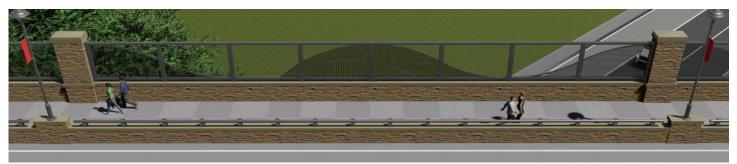
The elements in the design recommendations include:

- Exterior barrier wall enhancements
- Traffic barrier wall enhancements between travel lanes and pedestrian areas
- Columns (above bridge deck)
- Railings / fencing
- Specialty paving
- Lighting (commercial) and conduit
- Colors and finishes

Typical Renderings of Bridge Aesthetics







Roberts Drive requested betterments included:

- 12' multi-use path on south side
- Interior barrier wall separation on south side
- · Pedestrian lights on interior barrier wall on south side
- Utility conduits
- Decorative fencing/railing

GDOT will follow the guidelines for the side path extension up to and over the SR 400 express lanes bridge replacement project. Power poles and utilities relocation in these segments will be part of GDOT construction.

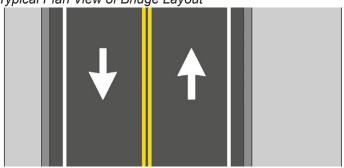


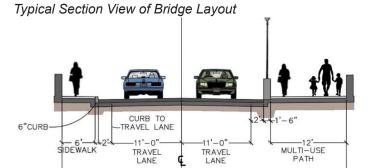






Typical Plan View of Bridge Layout





CITY OF SANDY SPRINGS

01.3 GOALS AND OBJECTIVES



INCREASE ACCESS TO CHATTAHOOCHEE RIVER NATIONAL RECREATION AREA

Improve access to the national recreation area with direct trail connection to Roberts Drive and the City of Sandy Springs



FILL GAPS IN ACTIVE TRANSPORTATION NETWORK

Expand the City of Sandy Springs multi-modal transportation system (walking and biking) Provide non-automobile access to Chattahoochee River National Recreation Area and other destinations along Roberts Drive



IMPROVE MULTI-MODAL SAFETY

Provide safe walking and biking routes within the City of Sandy Springs Reduce crashes between automobiles, pedestrians, and cyclists Help eliminate conflicts between modes of travel



REDUCE VEHICLE MILES TRAVELED

Improve air quality
Reduce car-dependence within the City of Sandy Springs



PROMOTE HEALTHY LIFESTYLES

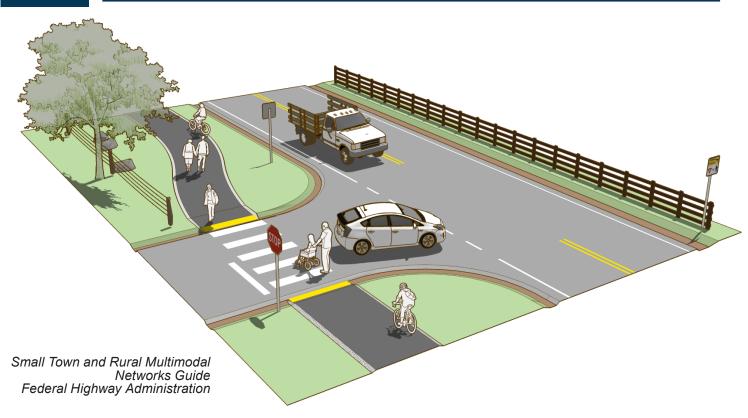
Promote physical and mental health benefits through exercise Promote social interaction Promote active commuting



PROTECT NATURE AND ENVIRONMENTAL RESOURCES

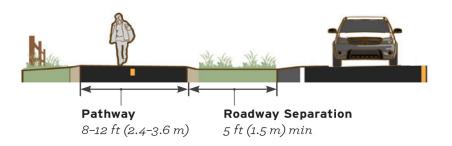
Improve access to nature Encourage environmental education

01.4 WHAT IS A SIDE PATH?



A side path is bi-directional shared use path located adjacent and parallel to a roadway. Side paths can offer a quality experience for users, including pedestrians and cyclists, while utilizing space within the right-of-way. Typical features that denote a side path include an 8-12 ft. path depending on available space, to allow for bi-directional traffic to pass by comfortably. The separation distance between traffic and the path should be no less than 5 ft. In areas where 5 ft. cannot be accomplished, a raised buffer such as shrubs or a guardrail should be provided for protection. This side path will increase the city of Sandy Springs' greater network of pedestrian and cyclist connectivity, while encouraging bicycling and walking within the Roberts Drive neighborhood.

Considerations for a side path are requirements for a wide shoulder or sufficient right-of-way to provide pedestrian separation from the roadway.

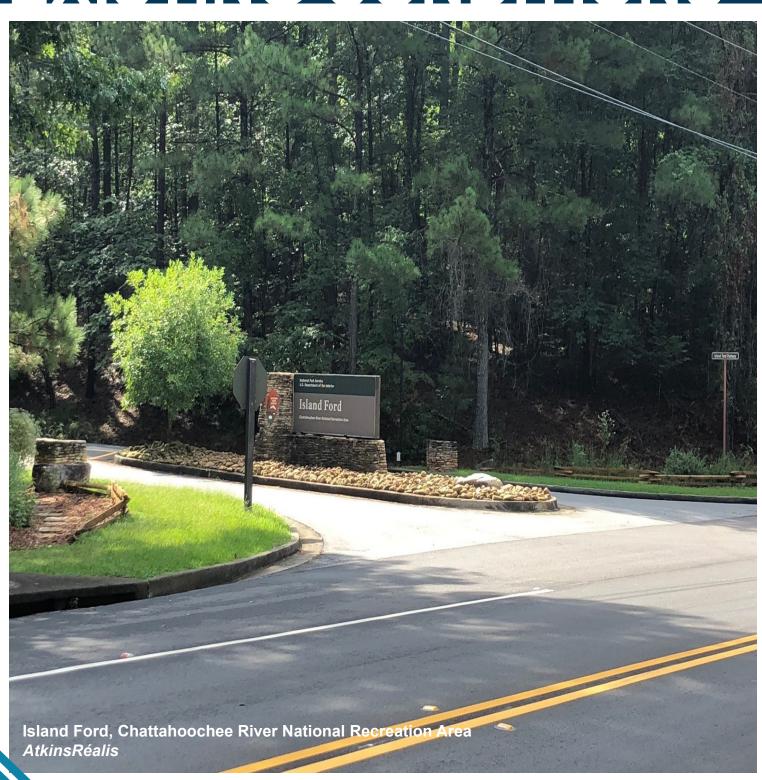


Typical Side Path Section Small Town and Rural Multimodal Networks Guide Federal Highway Administration

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS

SANDY SPRINGS

02 EXISTING CONDITIONS



- **EXISTING INFRASTRUCTURE** 02.1
- 02.2 **FUTURE CONNECTIVITY**
- **LAND USE AND ZONING** 02.3
- 02.4 **ECOLOGICAL REPORT**
- **CULTURAL RESOURCES REPORT** 02.5

02.1 EXISTING INFRASTRUCTURE

SIDEWALKS

Existing pedestrian sidewalks are located predominately adjacent to developed residential and commercial areas along Roberts Drive. Many gaps exist in the sidewalk network leaving pedestrians to travel on the shoulders of the roadway in many locations. Safe passageways and crossings are critical for the safety of the pedestrian. Although speed limits are posted at 35 mph, many speeds exceed the posted limit and visibility around curves is reduced, shortening reaction times for both vehicles and pedestrians to avoid dangerous conditions.



Gaps in Sidewalk Network
Preliminary Alternatives Assessment





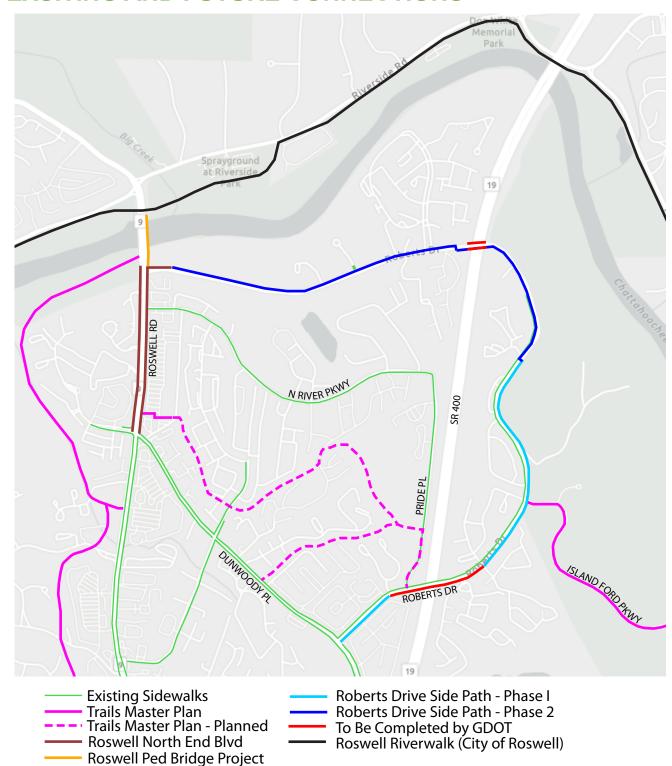
UTILITIES

Utility locations are considered in the design process. Fire hydrants may be relocated along with power poles and light poles adjusted, as needed, to accommodate the new side path and provide for pedestrian safety.



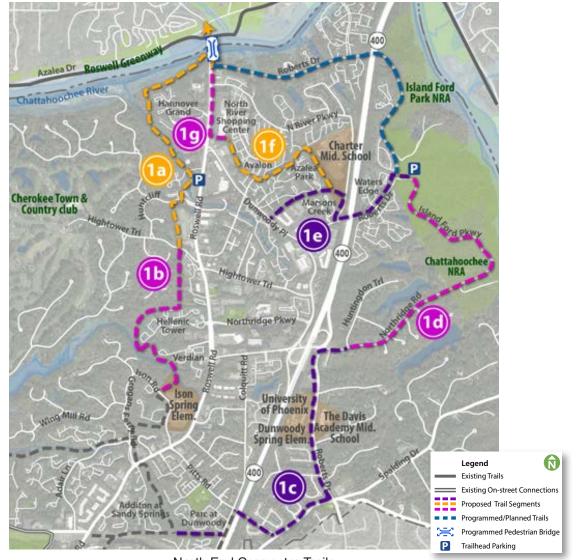
02.2 FUTURE CONNECTIVITY

EXSITING AND FUTURE CONNECTIONS



The Chattahoochee River National Recreation Area (CRNRA) has parks located within the city of Sandy Springs. The CRNRA is the largest urban green space environment and recreational jewel of the Atlanta Metropolitan region. The park is made up of 15 distinct and pristine land-based units surrounded by one of the fastest growing regions in the nation. Island Ford, located along Roberts Drive, is the most visited site among the CRNRA and is home to the park's headquarters. By creating safe and comfortable linkages, more residents within the city will have access to the public parks by foot or bike.

The Roberts Drive side path project intersects with the North End Connector trail project described in the Trails Master Plan beginning at Spalding Drive and ending at the Chattahoochee River, connecting North End residents to commercial hubs, schools, recreation areas and the existing Roswell Greenway over the Chattahoochee River. The planned pedestrian bridge over the Chattahoochee River will provide connections to the Roswell Riverwalk.

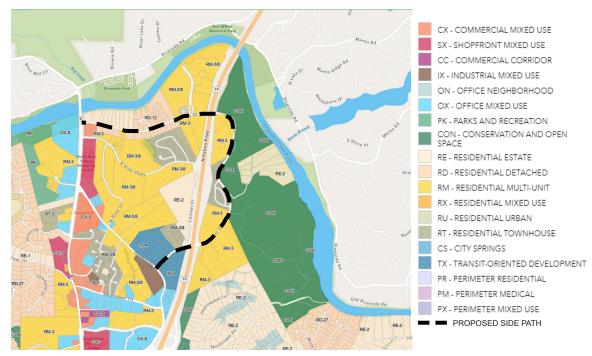


North End Connector Trails
Sandy Springs Trail Master Plan and Implementation Strategy

ROBERTS DRIVE SIDE PATH SCOPING REPORT

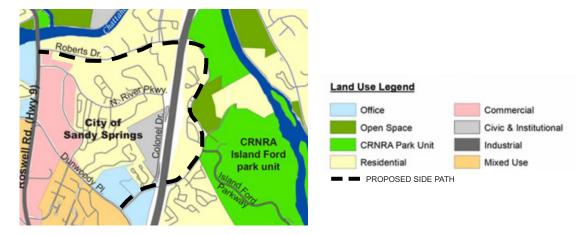
02.3 LAND USE AND ZONING

COMMUNITY DEVELOPMENT MAP



The City of Sandy Springs adopted a new development code and zoning map in August 2017. The above map shows the zoning around the proposed side path. The corridor is primarily residential townhouses (RT-3), residential multi-unit (RM-3), conservation and open space (CON), residential detached (RD-18, RD-27), and residential estate (RE-1). The northern end of Roberts Drive has a commercial mixed-use property (CX-3) on the opposite side of the street as the proposed trail. The southern end of Roberts Drive has transit-oriented development (TX-4) on either side of the roadway.

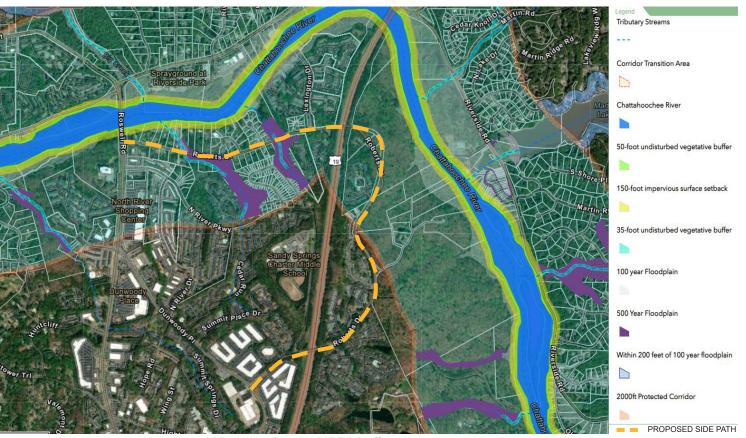
LAND USE MAP



The Roberts Drive corridor is primarily residential, scattered with some single-family homes, and multi-family townhomes and apartment complexes. The corridor also has some areas of commercial on the northern end of the project, fronting Roswell Road. The southern end of Roberts Drive where it meets Dunwoody Place is primarily single-story office buildings.

02.4 METROPOLITAN RIVER PROTECTION ACT

The Atlanta Regional Commission (ARC) Metropolitan River Protection Act (MRPA) was adopted by the Georgia General Assembly in 1973 to protect a 48-mile stretch of the Chattahoochee River between Buford Dam and Peachtree Creek by creating a 2,000 ft. buffer along both banks of the river and its impoundments. The Act was amended in 1998 to extend the Corridor an additional 36 miles to the downstream limits of Fulton and Douglas counties. A 2023 amendment to the Corridor Plan was added to describe planting restoration and other clarifications. MRPA is designed to protect the water quality and environs of the Chattahoochee River. Phase 2 of the Roberts Drive side path falls within the limits of the 2,000 ft. protected corridor of the MRPA and will require further study to determine how potential impacts to the river will be addressed to comply with land-disturbing limits of the policy standards.



Map showing the NRPA buffers.

Atlanta Regional Commission

All land-disturbing activity in the corridor must be reviewed, approved, and certified for consistency with Corridor Plan standards. Consult the MRPA Rules and Regulations for procedures and requirements for corridor reviews. The local government will determine if a review is needed.

23

02.5 ECOLOGICAL REPORT

As part of the project planning process, a desktop review was conducted to identify potential State and Federal Waters within the proposed project limits. To conduct this desktop review, the following resources were utilized: USGS topographic maps, aerial imagery, National Wetland Inventory (NWI) maps, Federal Emergency Management Agency (FEMA) Floodplain Maps, and soil maps. Additionally, Google Earth Streetview was utilized to visualize approximate conditions on the ground. No field visits have been conducted as part of this desktop review. Based on the data available, there are two streams, one open water, and one additional buffer anticipated to be present within the project limits. Refer to the full Ecological report located in the appendix.

Although the Chattahoochee River is not located within the project limits, it is considered a trout stream and would be subject to a 50' state-mandated stream buffer. All other channels with perennial or intermittent flow and open waters within Fulton County are not considered trout waters and are subject to a 25' state-mandated stream buffer. Channels with ephemeral flow are also not considered trout waters and are not subject to state-mandated buffers in Fulton County. The City of Sandy Springs requires a minimum of 75' buffer along designated streams and bodies of water. Of the 75', 50' must remain undisturbed, while the other 25' property owners can remove vegetation and grading.

Location 1: Stream near 9679 Roberts Dr.

A 75 ft. city mandated stream buffer would apply.

Location 2: Stream near 9500 Roberts Dr
A 75 ft. city mandated stream buffer would apply.



The map above shows the approximate location of the open waters and streams within the project limits.

The proposed alignment for the side path will traverse two state-mandated stream buffers on Phase 2 of the project with proposed boardwalks to stay within the Federal guidelines for minimal disturbance.

02.6 CULTURAL RESOURCES REPORT

As part of the project planning process, a cultural resources desktop screening was conducted. The review included the National Register of Historic Places (NHRP), National Historic Landmarks, and historic and archaeological properties included in Georgia's Natural, Archaeological and Historic Resources Geographic Information System (GNAHRGIS) database.

One NRHP (National Register of Historic Places) listed resource is located within the proposed projects area of potential effects. This resource, the Isaac Roberts House, is located at 9725 Roberts Drive. The house was built in 1894 and listed on the NRHP in 2008.

Isaac Roberts was a chief engineer for the Roswell Railroad and later founder of Roswell Bank. The railroad was a three-foot narrow-gauge railroad that ran south of Roswell GA to Chamblee from 1881 to 1921 as a passenger and freight carrier. The Roswell terminus was located south of the Chattahoochee River near Roberts Drive. Ike (Isaac) Roberts was an employee of Southern Railway at the time the company decided to create the Roswell Railroad. Roberts participated in the grading and track laying for this new line. He also purchased 650 acres of land at the northern terminus, built a train station, and leased it to Southern Railway. After completion of the line, he stayed on as the engineer and was the only person that worked in that capacity until the closing of the line. His home remains on Roberts Drive (his namesake in the Sandy Springs community) (Wikipedia).

Some features that potentially contribute to the eligibility of the Isaac Roberts House are located within and outside the existing right-of-way of Roberts Drive. These features include trees, vegetation, fence, brick pavers, and stone pillars.

Items in the right of way could be impacted. Trail narrowing will occur along this property to minimize potential impacts.



Isaac Roberts Property Signage and Columns Preliminary Alternatives Assessment



Isaac Roberts House
Preliminary Alternatives Assessment

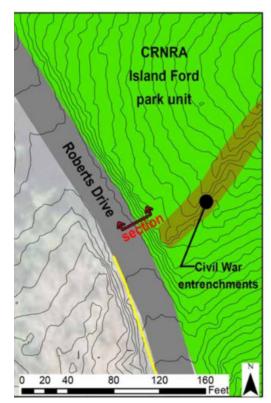
02.6 CULTURAL RESOURCES REPORT

An archaeological resource, Civil War military entrenchment line (site number 9FU753), is located on top of a slope and was identified within the projects area of potential effects. "This line, entrenched by Federal troops during the Atlanta campaign (5-7-1864 to 9-2-1864) immensely strengthened General W. T. Sherman's hold on the Chattahoochee River's south bank. This causing the Confederate Army to abandon its river line and withdraw south of the river." (Hitt 1991)

This site is located on NPS property and does not extend into the existing right-of-way.

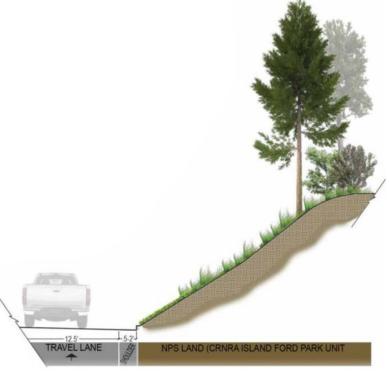
Three other archaeological resources were identified in the report and determined to not be affected by the project limits. Fourteen additional resources 50 years of age or older are located within the project's area of potential effects. The resources were identified via the Fulton County tax assessor's records.

Refer to the appendix for the full cultural resources report for more information on these sites.



Plan view of Civil War Trench Location Preliminary Alternatives Assessment

26



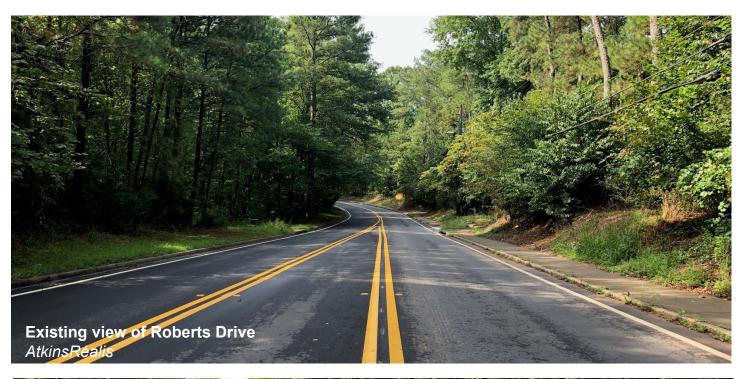
Section view of Civil War Trench Location Preliminary Alternatives Assessment

THIS PAGE LEFT INTENTIONALLY BLANK

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS 27

03 TECHNICAL ANALYSIS

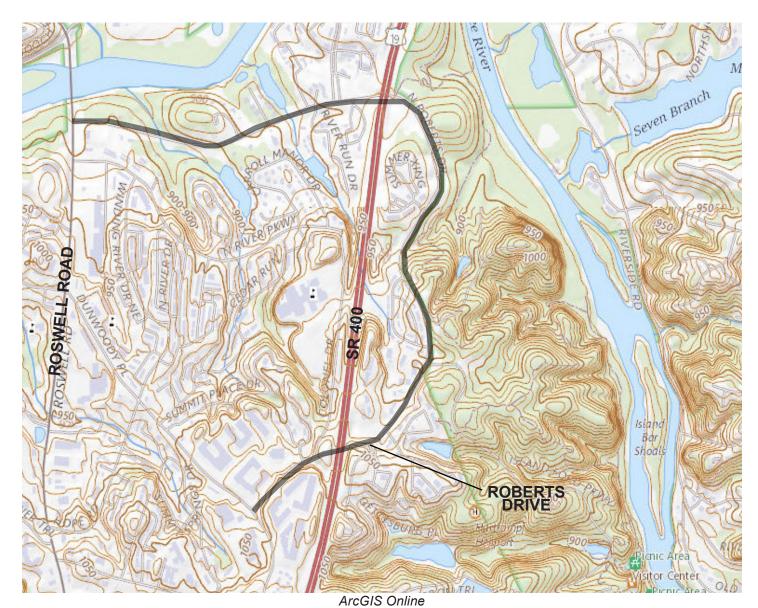






03.1 ELEVATION MAP

03.2 SLOPE ANALYSIS MAP



The elevation of Sandy Springs, Georgia is 323m / 1060feet. The primary coordinate point for Sandy Springs is located at latitude 33.9243 and longitude -84.3785 in Fulton County. The formal boundaries for the City of Sandy Springs encompass a land area of 37.66 sq. miles and a water area of 0.87 sq. miles.

Fulton County's elevation is 1,093 feet. The elevation of the area encompassing Roberts Drive spans between 900 ft. elevation at the northern end (Phase 2) and 1050 ft. elevation at the southern end (Phase 1).

Certain areas adjacent to the roadway within the right of way have steep grades and will require walls for the side path to traverse the adjacent terrain.

03.2 SLOPE ANALYSIS MAP



Slopes in the right-of-way along Roberts Drive side path construction zones are moderate with steeper grades along portions of the commercial areas (Phase 1) and adjacent to some residential areas along phase 2. These areas will require walls for the side path to traverse the terrain.

04 DESIGN RECOMMENDATIONS

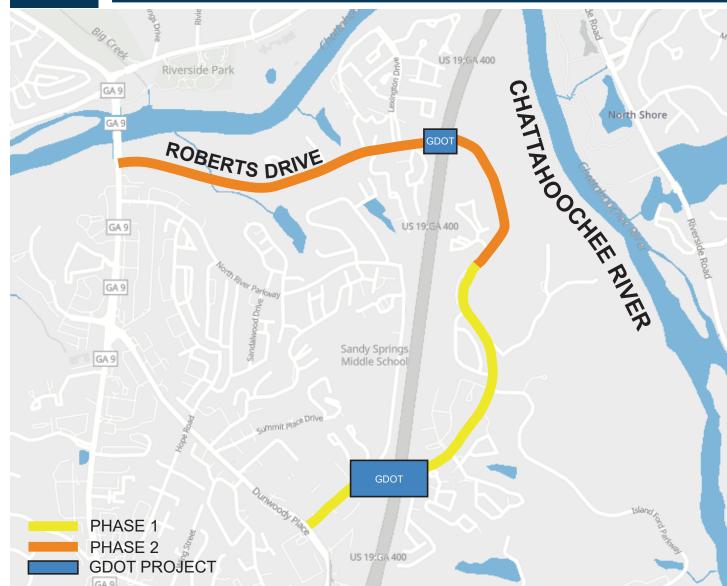






04.1	PHASING PLAN
04.2	SITE AMENITIES
04.3	EASEMENTS
04.4	PHASE ONE PLANS
04.5	PHASE TWO PLANS
04.6	PERSPECTIVES
04.7	STREAM CROSSING ALTERNATIVES COMPARISON
04.8	MID-BLOCK CROSSINGS SIGHT DISTANCE STUDY

04.1 PHASING PLAN



The side path is divided into a two-phase construction project.

Phase 1 (highlighted in yellow) will begin at Dunwoody Place (STA 0+00) and travel north to the start of the GDOT bridge replacement project over SR 400 (STA 8+12). The first phase will pick back up on the other end of the GDOT project (STA 22+17) and continue to a midblock crossing near Summer Crossing (STA 53+10). The phase 1 end point was chosen at the mid-block crossing near Summer Crossing to provide a connection point to the existing sidewalk locations and to provide residents along the opposite side of Roberts Drive access to the first phase of trail. Phase 2 (highlighted in orange) will begin at the midblock crossing (53+10) and travel north on the west side of the roadway to the limits of the GDOT SR 400 underpass bridge replacement project (STA 71+95). This phase will pick back up on the other end of the GDOT project (STA 75+38) and cross to the north side of Roberts Drive. The path will continue to Roswell Rd, where the side path ends (STA 119+50).

04.2 SITE AMENITIES





Pedestrian Light

Intersection Light



Metal Bollards

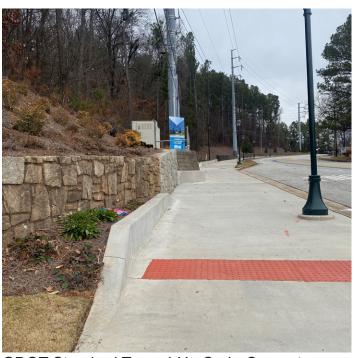


Rectangular Rapid Flashing Beacons (RRFB)

04.2 SITE AMENITIES



GDOT Standard Gravity Wall, Stone



GDOT Standard Turned-Up Curb, Concrete



GDOT Standard 42" Cabled Handrail, Steel



GDOT Standard Poured-in-Place Wall, Stone

04.3 EASEMENTS

Temporary Construction Easement:

5 ft. will be provided during construction off the back of the side path. In areas where this space is not available within the right-of-way, up to 5 ft. temporary construction easement will be provided. In areas with a gravity wall, a minimum of 10 ft. will be provided from the back of the wall during construction. In areas where this space is not available within the right-of-way, up to 10' temporary construction easement will be provided. These easements are shown in Blue on the plans.

Permanent Easement:

A permanent easement will be provided at the proposed culvert extension required near station 51+00. These easements are shown in **Green** on the plans.

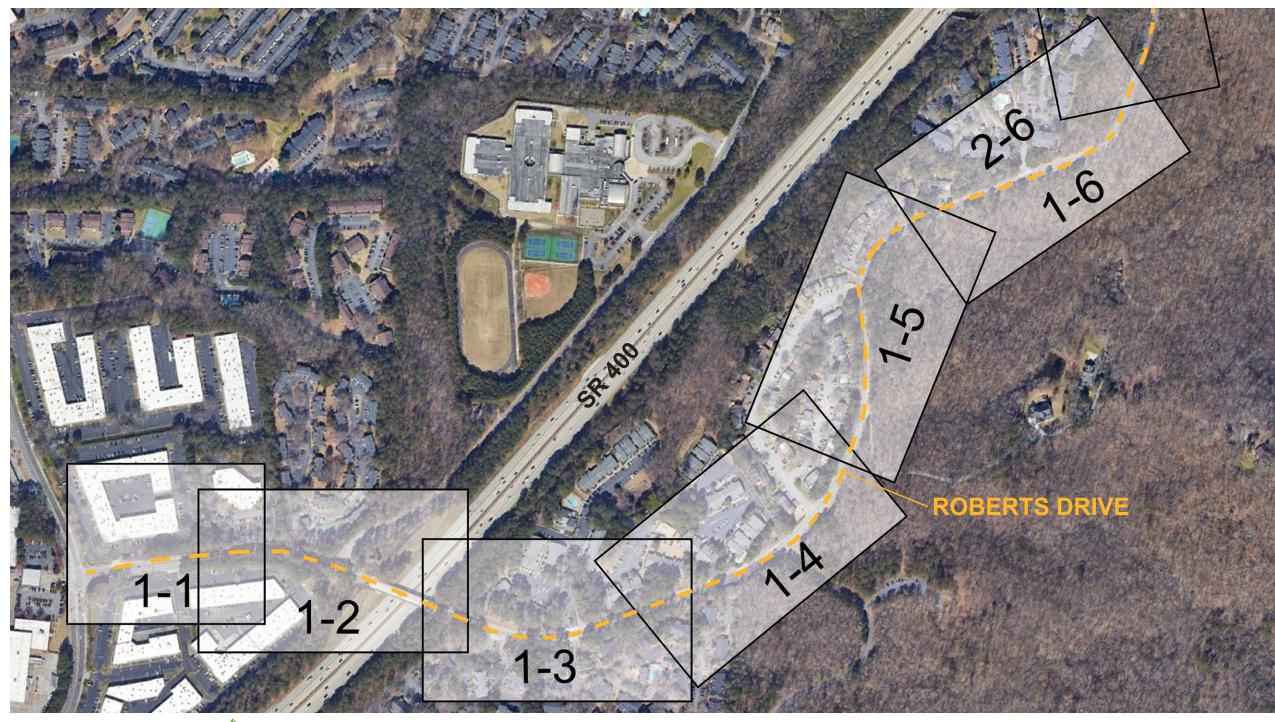
Temporary Driveway Easement:

In areas where driveway reconstruction is required, a temporary driveway easement will be provided. The size of each easement varies based on requirements to rebuild each drive. These easements are shown in **Orange** on the plans.

Required Right of Way:

Additional ROW will need to be acquired in areas to meet the minimum space required for the side path. Required ROW is shown in **Red** on the plans.

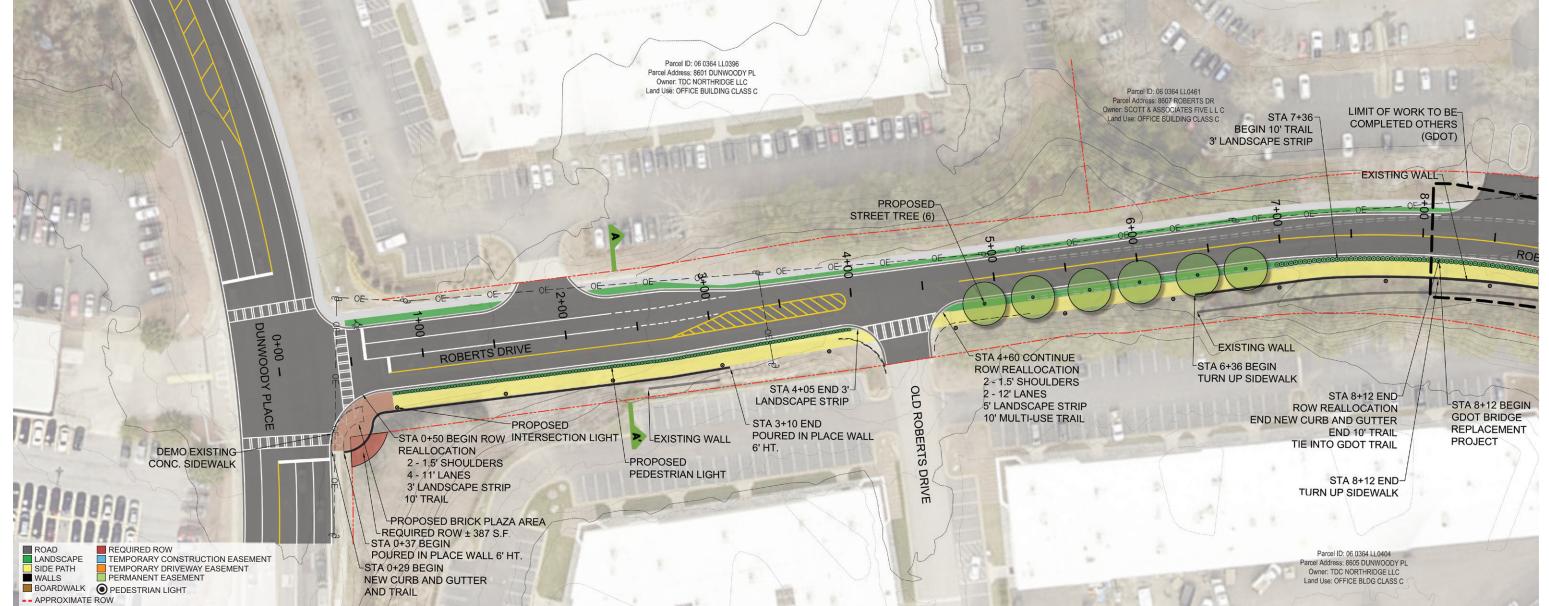
04.4 PHASE 1 OVERVIEW





Phase 1 begins at Dunwoody Place and continues approximately 5,200' before ending near Summer Crossing. This phase includes a GDOT bridge replacement project and is first to be reconstructed, of the two along Roberts Drive. Before the SR 400 express lane bridge, there are office complexes and apartment communities. After the bridge, adjacent properties are multi-family residential and the Chattahoochee River National Recreation Area (CRNRA). There are cultural and historical areas noted in this phase.

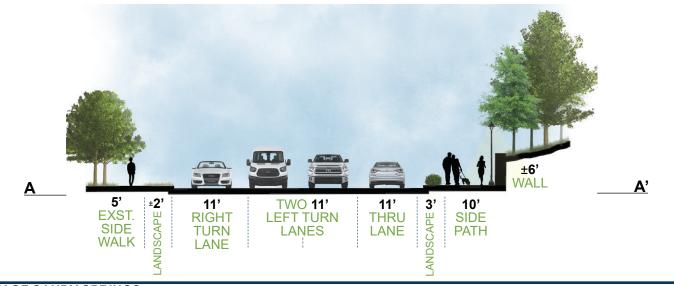
Adjacent steep grades along this corridor will require reallocation of right of way and wall construction which will determine the variations in the landscape strip and side path widths. Three new midblock pedestrian crosswalks are proposed in this phase to provide a safe crossing to the side path. Temporary and permanent easements, along with power pole relocations are included in this phase.



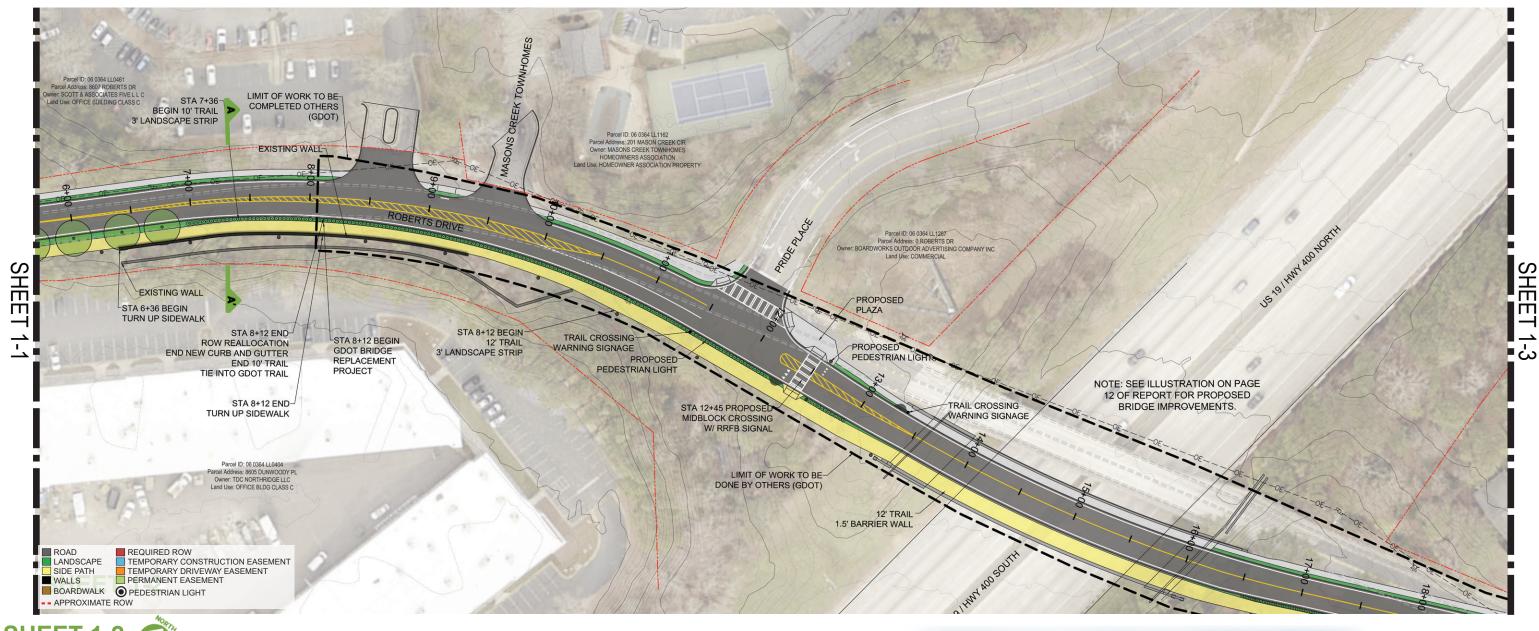


Phase 1 of the Roberts Drive side path begins at Dunwoody Place. A small plaza space will be constructed at the southeast corner of the intersection. This plaza will welcome users to the start of the path and allow extra room for pedestrians waiting to cross the street. Additional right-of-way (ROW) will be required to construct the plaza space.

This section includes a reallocation of right of way to allow for extra room for the side path. The path begins 10 ft. wide with a 3 ft. landscape strip and travels northeast along Roberts Drive. Due to steep grades along the shoulder, a retaining wall will travel the first 250' of the path. As the path passes the first drive, Old Roberts Drive, the landscape strip widens to 5 ft. with a 10 ft. path. The Georgia Department of Transportation (GDOT) SR 400 bridge replacement project begins at STA 08+12. Phase 1 will tie into the improvements completed in the GDOT project.

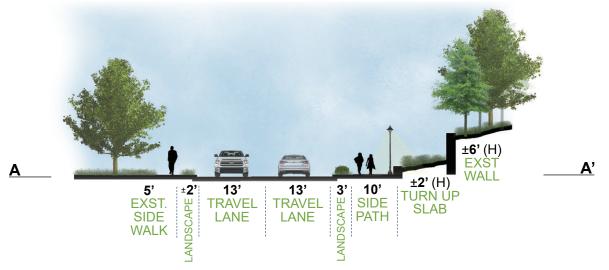


04.4 PHASE 1 (STA 8+00 TO 18+00)

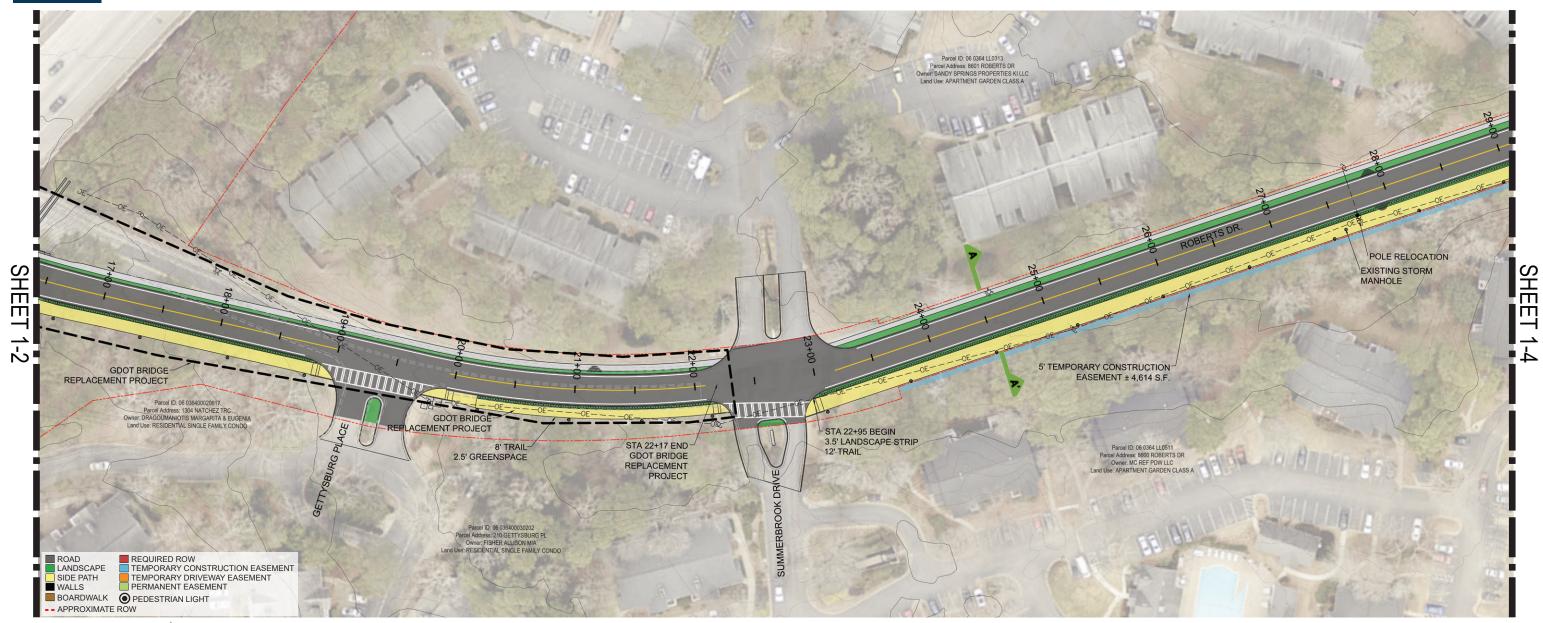


SHEET 1-2 NOT TO SCALE

This segment of the Roberts Drive side path will be constructed within the GDOT bridge replacement project. Once bridge construction has been completed, these plans call for adding in a mid-block crossing where Pride Place meets Roberts Drive. Along with this, new ADA ramps will need to be built, signage, and striping will occur. Adequate warning signage for the trail crossing will be necessary, as well as the rapid rectangular flashing beacons at the crosswalk. Refer to section 04.8, mid-block crossings sight distance study, for input on where the mid-block crossing should be located. Phase 1 of construction will also include adding pedestrian lighting and street trees in this section of the side path, where it was not included in the GDOT project.

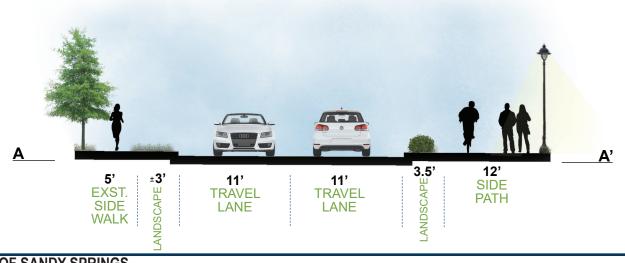


04.4 PHASE 1 (STA 18+00 TO 29+00)

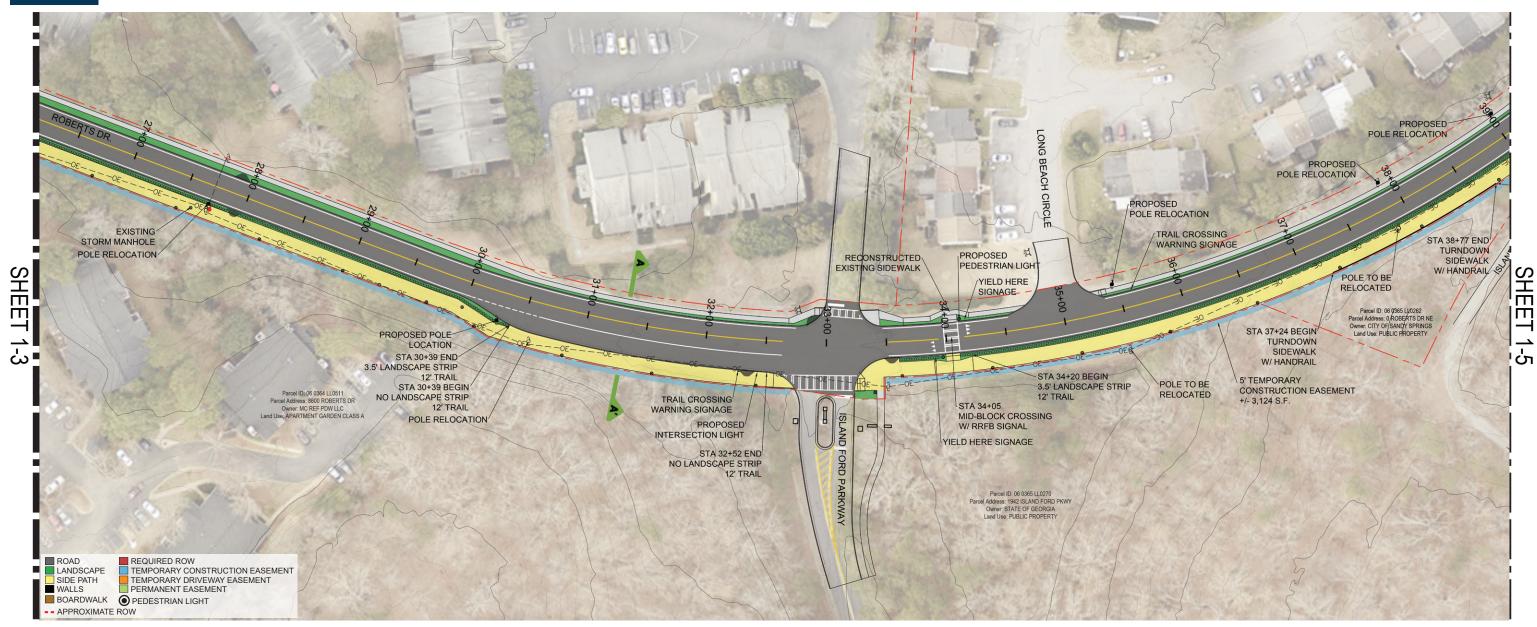




This segment continues at STA 18+12, within the GDOT SR 400 bridge replacement project, which ends at STA 22+17, adjacent to Summerbrook Drive. The side path will be constructed with the bridge in the area within the GDOT project limits. The city will resume side path construction beginning at STA 22+17. Section A-A' shows path width of 12 ft., with a 3.5 ft. landscape strip. One power pole at station 27+72 will be relocated from the proposed path alignment to the opposite side of Roberts Drive. A 5 ft. temporary construction easement (TCE) will be required in this segment for construction access.



04.4 PHASE 1 (STA 29+00 TO 38+00)

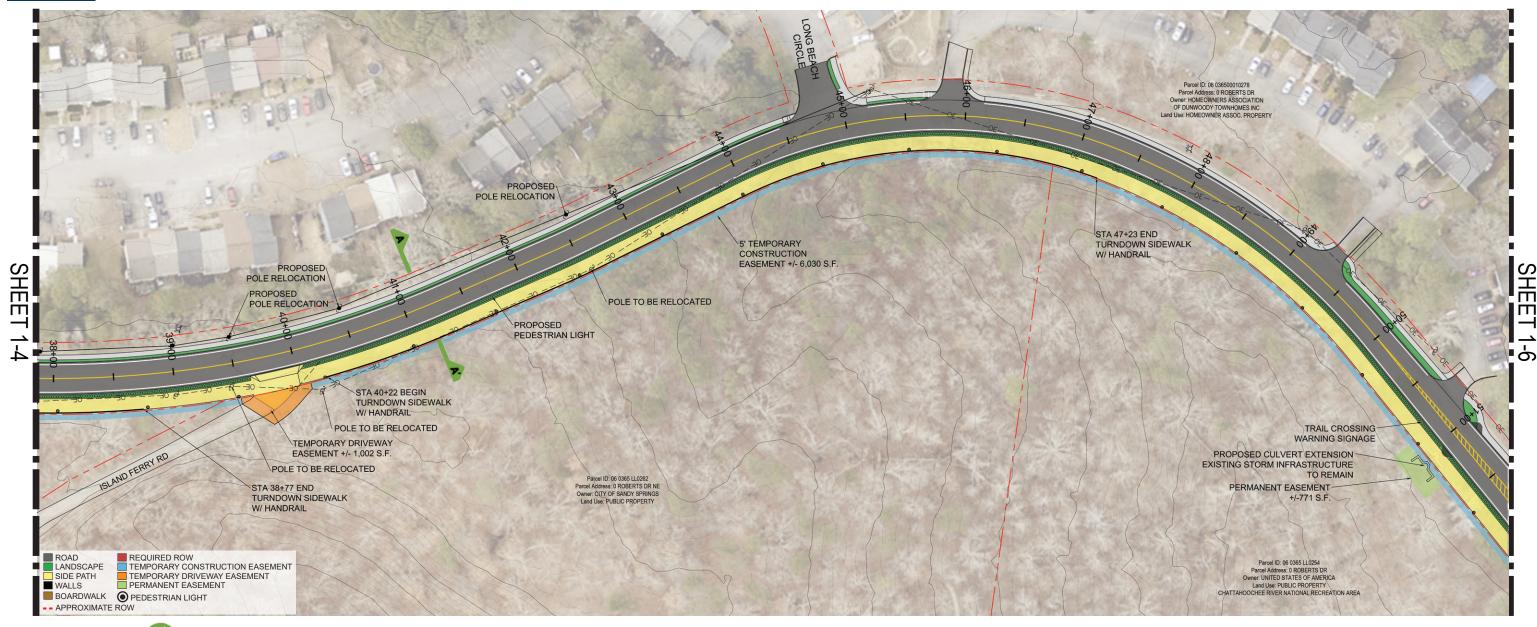




This segment continues at STA 29+00 and ends at STA 38+00. The Chattahoochee River National Recreation Area (CRNRA) is a popular destination, and it is important to provide safe pedestrian access to the entrance drive, at Island Ford Parkway. A second mid-block crossing provides pedestrian crossing from the residential areas to the park. Refer to section 04.8, mid-block crossings sight distance study, for input on where the mid-block crossing should be located. Section A-A' shows the 12 ft. path with no landscape strip, as the deceleration lane provides a buffer from the moving traffic. The current deceleration lane is approximately 200 ft long, which can be reduced to minimize construction easements required. Past Island Ford Parkway, the path will transition back to a 12 ft wide with a 3.5 ft. greenspace. An adjacent slope on CRNRA property requires a turn-down curb with handrail. Four power poles will be relocated from the trail side of the roadway. A 5 ft. temporary construction easement will be required in this segment for construction access.



04.4 PHASE 1 (STA 38+00 TO 51+00)

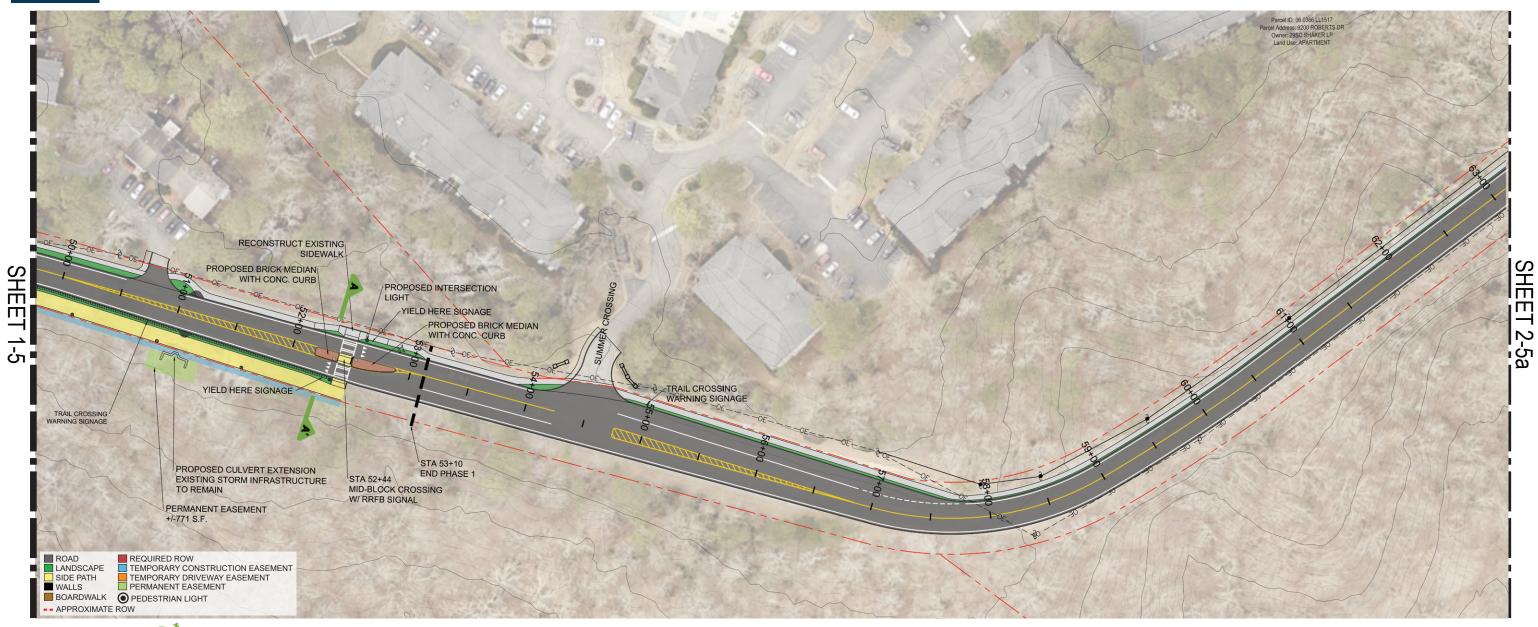




This segment continues at STA 38+00 and ends at STA 51+00. The path is adjacent to the CRN-RA property at 12 ft wide with a 3.5 ft. greenspace. Section A-A' shows a turn-down curb with handrail along an area where the shoulder within the ROW has steep grades going downhill. Two power poles will be relocated from proposed path alignment to the opposite side of Roberts Drive. A 5 ft. temporary construction easement will be required in this segment for construction access. One temporary driveway easement will be needed to reconstruct Island Ferry Road. A permanent easement will be required for a stormwater culvert extension to accommodate the path width.



04.4 PHASE 1 (STA 51+00 TO 52+45)





This segment continues at STA 51+00 and Phase 1 ends at STA 53+10. The path is adjacent to the CNRA property at 12 ft wide with a 3.5 ft. landscape strip. The third mid-block crossing in Phase 1 is located at STA 52+44 with a pedestrian refuge island, to be constructed with a concrete curb and brick. Refer to section 04.8, mid-block crossings sight distance study, for input on where the mid-block crossing should be located. Section A-A' shows the mid-block crossing, with rectangular rapid flashing beacon signage, and the median island in the center of the roadway. This crossing will provide safe access to the path from the residential areas across Roberts Drive. A 5 ft. temporary construction easement is shown for construction access. Phase 2 of the side path will conclude at the mid-block crossing constructed in phase 1.



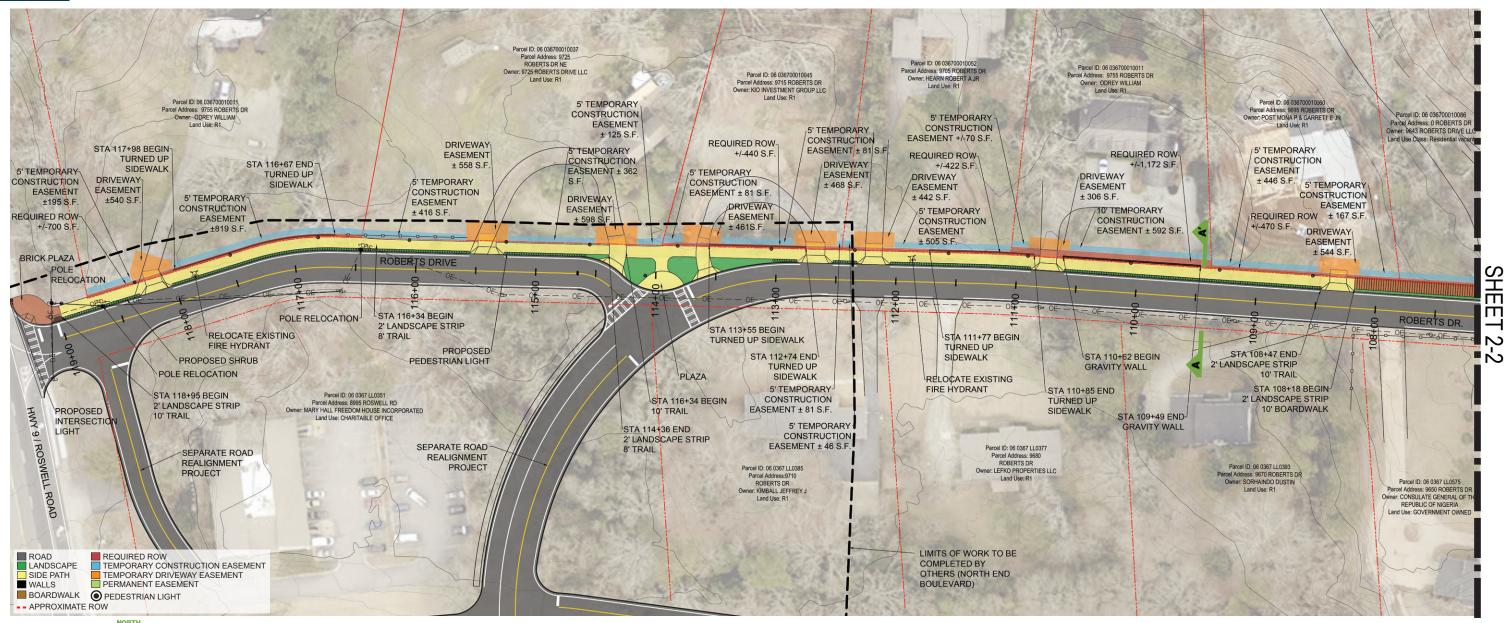
04.5 PHASE 2 OVERVIEW



Phase 2 begins at Roswell Road (STA 119+50) and continues approximately 6,700' before ending at STA 53+10, where the side path will tie into the terminus of phase 1. This phase includes the second GDOT bridge replacement project, which will be reconstructed following the completion of the first bridge replacement along Roberts Drive. From Roswell Road, Roberts Drive travels through a residential area, mainly with single-family residential and multi-family complexes. There are cultural and historical areas noted in this phase, as well as streams the path will cross.

Path and landscape strip widths through this phase will vary depending on roadway alignment, ROW space, and existing conditions. One mid-block crossing is proposed in Phase 2 near Lexington Drive, where the side path will crossover Roberts Drive. This phase calls for two pedestrian bridges/board-walks, as well as utility relocations.

04.5 PHASE 2 (STA 119+50 TO 107+50)

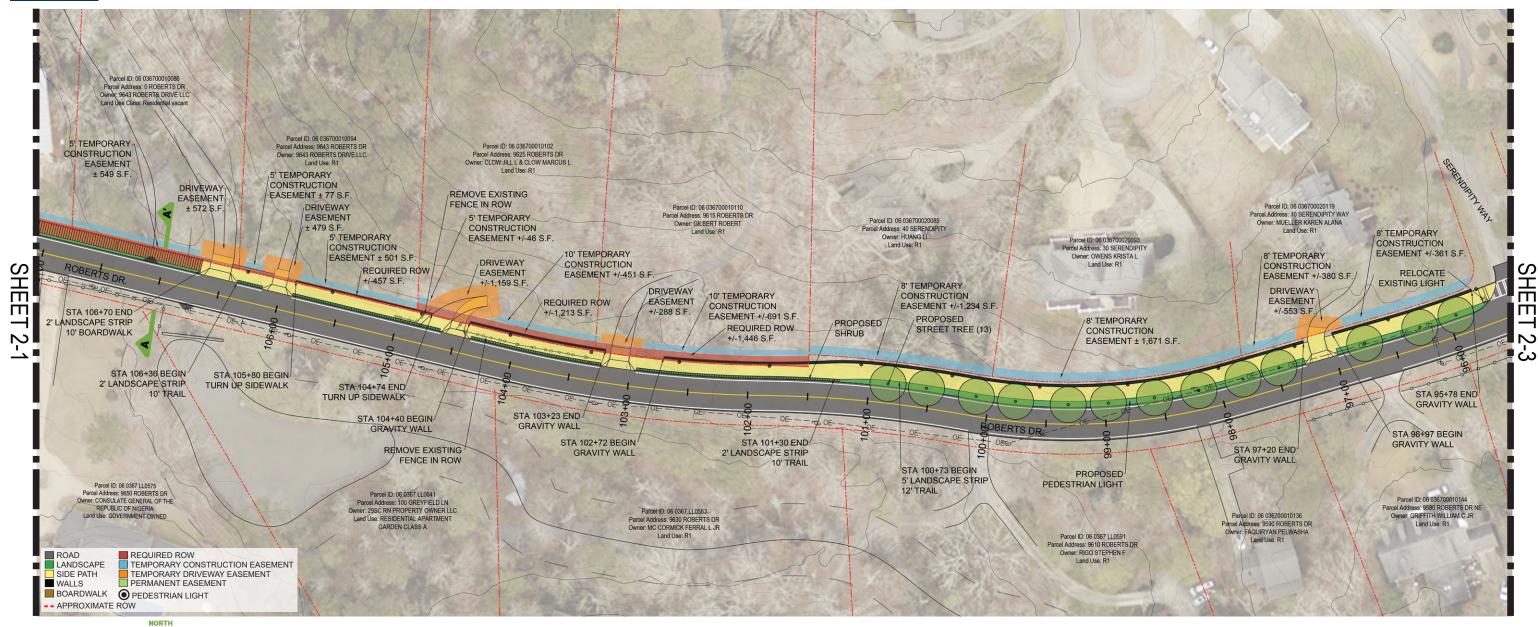




Phase 2 begins at Roswell Road, STA 119+50 to STA 107+50. The width begins as a 10 ft. wide path and a 2 ft. planted greenspace. After traveling approximately 300', the path narrows to 8 ft. wide, and the 2 ft. planted landscape strip continues. The 8 ft. path travels the length of the property located at 9725 Roberts Drive, the Isaac Roberts House, which is listed on the National Register of Historic Places. The path then transitions back to the 10 ft. wide path with a 2 ft. planted landscape strip. The North End Boulevard Scoping Study re-aligns Roberts Drive and focuses on improving the Roswell Road to Dunwoody Place Corridor. These improvements are outside of the Roberts Drive side path scoping study and are shown to inform the design. As the North End project advances, changes to this section of the side path may be nescessary. Topography changes will require walls within this segment. Additional right of way, temporary construction, and driveway easements will be required within this segment. A boardwalk is proposed to cross existing drainageway beginning at STA 108+18, and utility relocations will be required.

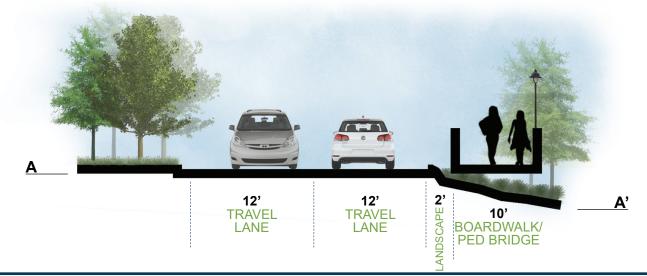


04.5 PHASE 2 (STA 107+50 TO 96+50)

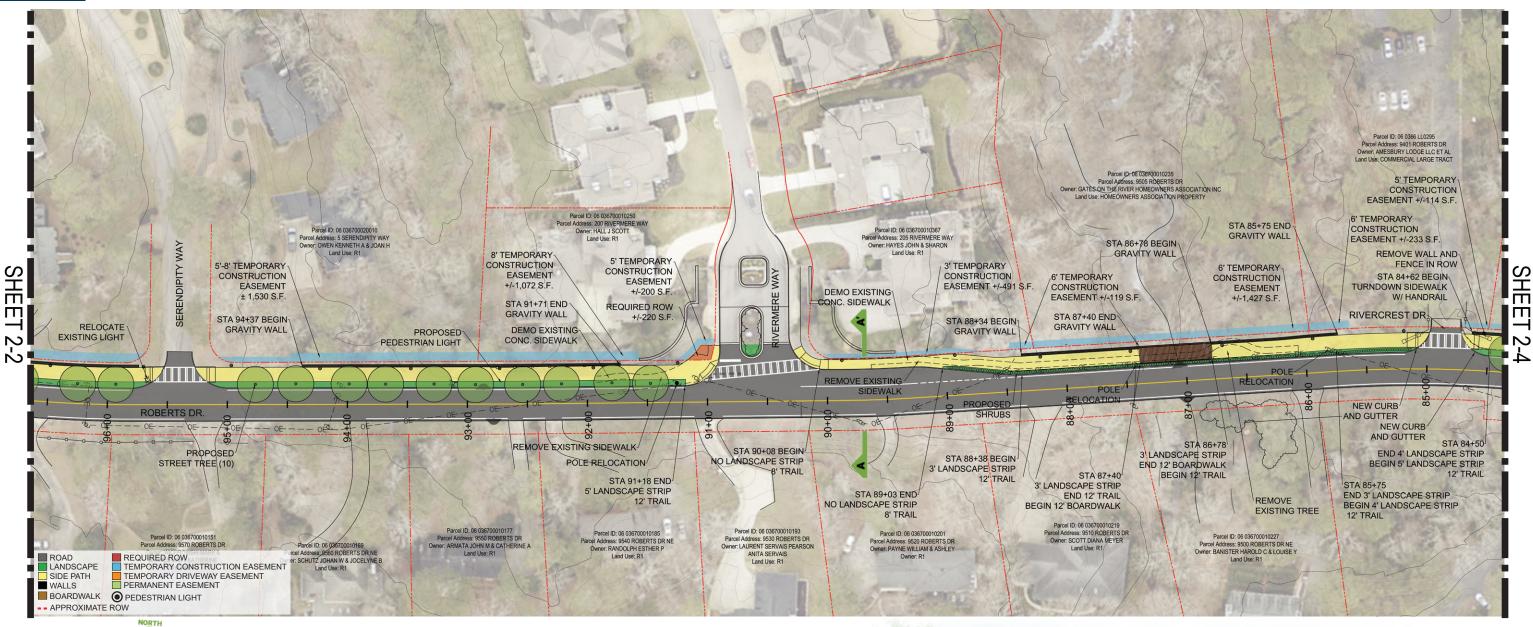




This segment continues at STA 107+50 and ends at STA 96+50. The boardwalk continues to station 106+70. A 75 ft. city-mandated stream buffer would apply, see section 02.4 ecological report for more information. Section A-A' shows the 10 ft. wide boardwalk/pedestrian bridge with a 2 ft. landscape strip buffer between the road. Refer to section 04.7, alternatives comparison, for water body crossing options. At station 101+73, the path transitions to 12 ft. wide with a 5 ft. greenspace, which will include street trees. Topography changes will require a turned-up curb and a gravity wall. Additional right of way, temporary construction, and driveway easements will be required within this segment. Utility relocations will be required.



O4.5 PHASE 2 (STA 96+50 TO 84+50)

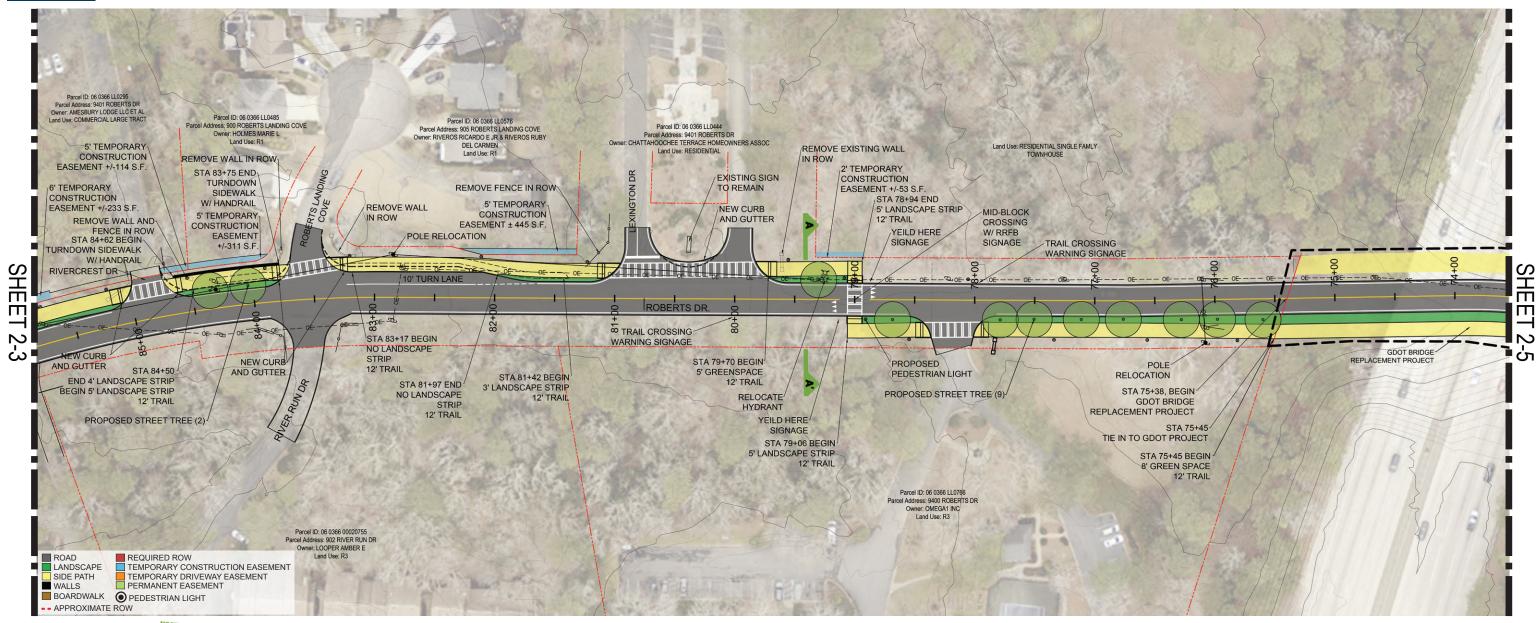




This segment continues at STA 96+50 and ends at STA 84+50. The path remains 12 ft. wide with a 5 ft. landscape strip. A second boardwalk/pedestrian bridge begins at station 87+40 and traverses the natural stream to STA 86+78. A 75 ft. city mandated stream buffer would apply, see section 02.4 ecological report for more information. Refer to section 04.7, alternatives comparison, for water body crossing options. Section A-A shows the 12 ft. path with no landscape strip, as the deceleration lane provides a buffer from the moving traffic. The path remains 12 ft. wide, and a 3 ft. landscape strip begins at STA 88+3. Topography changes will require two gravity walls. Additional right of way, temporary construction, and driveway easements will be required within this segment. Utility relocations will be required.

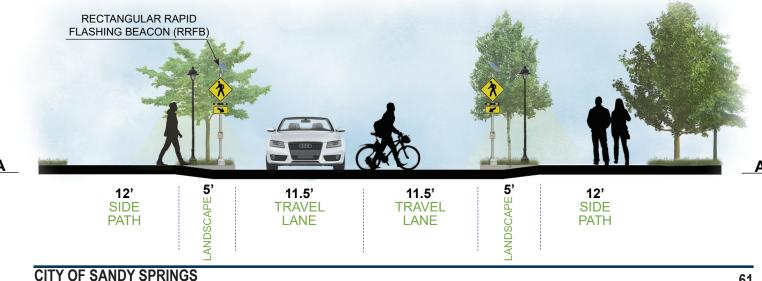


PHASE 2 (STA 84+50 TO 74+50)

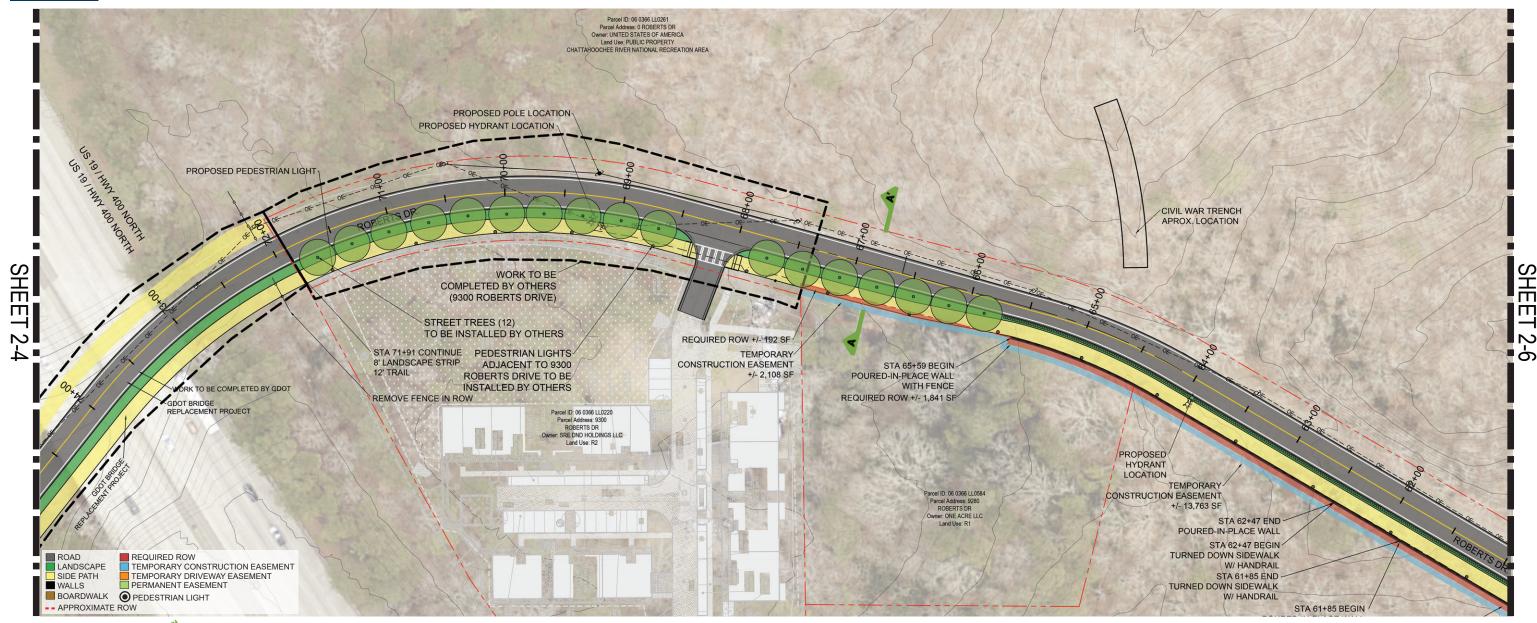




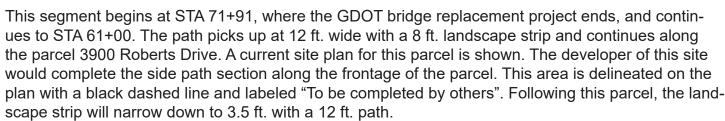
This segment continues at STA 79+06 through STA 73+50. A mid-block crossing will be located at STA 79+00, where the path will cross Roberts Drive and continue on the southside of the road. After crossing the road, the path continues at 12 ft. wide with a 5 ft. landscape strip, with street trees. This sheet ends at STA 75+36, where the GDOT bridge replacement project will begin. The bridge replacement project will provide a 12 ft. wide side path with a 8' landscape strip on both side of Roberts Drive. Temporary construction easements will be required on the north side of Roberts Drive. Utility relocations will be required. Section A-A' shows the mid-block crossing with rectangular rapid flashing beacons. There is no median at this crossing location.



04.5 PHASE 2 (STA 74+50 TO 61+00)



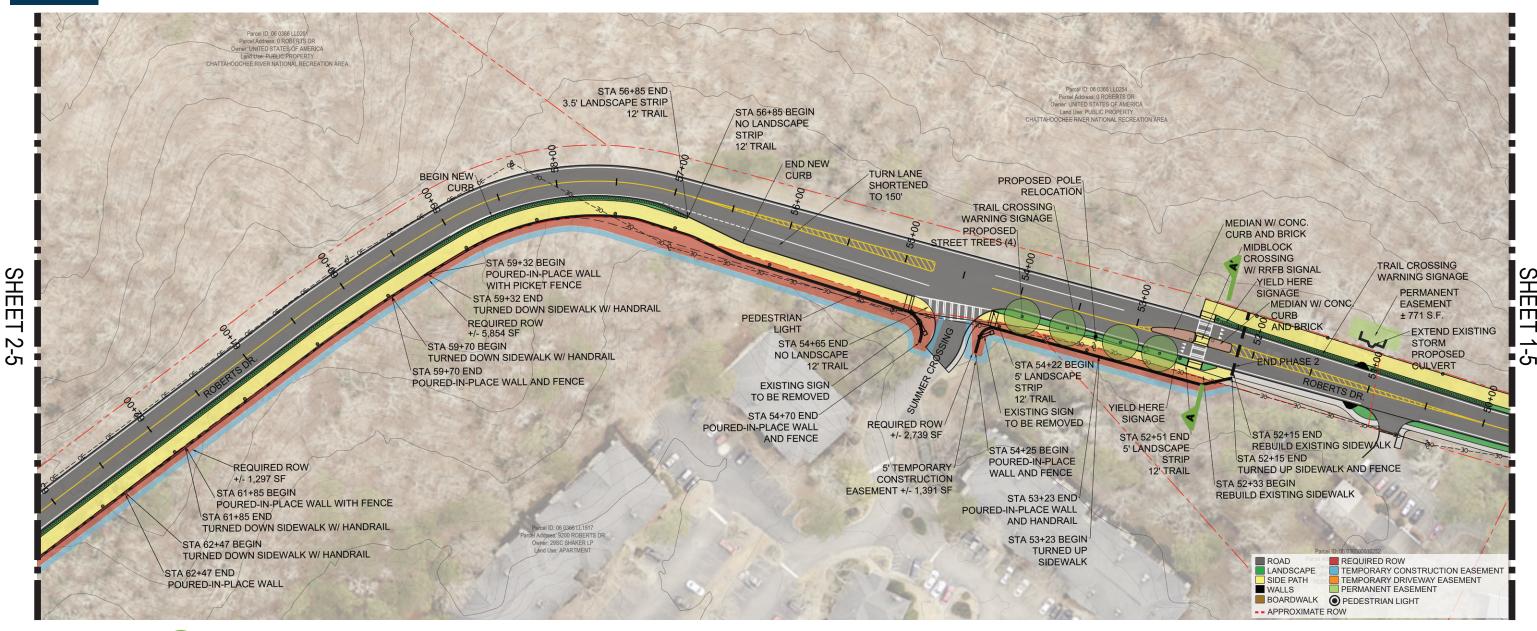




Topography changes will require a turned down sidewalk and a poured in place wall. Additional right of way and temporary consturction easements will be required within this segment. Utility relocations will be required. In the appendix, a version of this section of side path is available that does not show the site development at 6300 Roberts Drive.

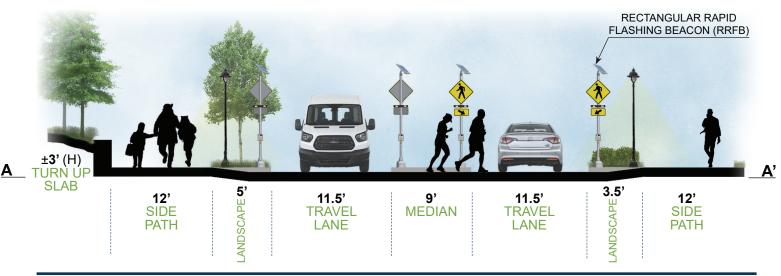


04.5 PHASE 2 (STA 61+00 TO 53+10)





This segment continues at STA 61+00 and ends at 53+10, the terminus of Phase 2. The path maintains a 12 ft. width and a 3.5 ft. landscape strip along the segment to station 57+10. A deceleration lane into Summer Crossing removes the landscape strip. The deceleration lane is shown as shortened from the existing length to 150 ft. with a 75 ft. taper. The trail crosses Summer Crossing, where the existing sidewalk will be removed, and the path will continue to the mid-block crossing constructed in phase 1 (Section A-A'). Topography changes will require a turned down sidewalk and poured in place walls. Additional right of way and temporary construction easements will be required within this segment. Utility relocations will be required.



04.6 PERSPECTIVES

ROBERTS DRIVE AT DUNWOODY PLACE (PHASE 1)

VIEW AT STA 0+00, LOOKING NORTHEAST TOWARDS ROBERTS DRIVE







VIEW AT STA 2+25, LOOKING SOUTHWEST TOWARDS DUNWOODY PLACE







04.6 PERSPECTIVES

ROBERTS DRIVE AT ISLAND FORD PARKWAY (PHASE 1)

VIEW AT STA 33+00, LOOKING NORTHEAST TOWARDS ISLAND FORD PKWY











ROBERTS DRIVE SIDE PATH SCOPING REPORT

CITY OF SANDY SPRINGS

04.6 PERSPECTIVES

MID-BLOCK CROSSING (PHASE 1)

VIEW AT STA 51+50, LOOKING NORTHEAST TOWARDS SUMMER CROSSING \(\int \)





VIEW AT STA 52+75 LOOKING SOUTHWEST TOWARDS THE PEDESTRIAN CROSSING





ROBERTS DRIVE SIDE PATH SCOPING REPORT

CITY OF SANDY SPRINGS

71

04.6 PERSPECTIVES

TYPICAL VIEW (PHASE 2)

VIEW AT STA 66+00











04.6 PERSPECTIVES

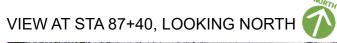
PEDESTRIAN BRIDGE/BOARDWALK (PHASE 2)



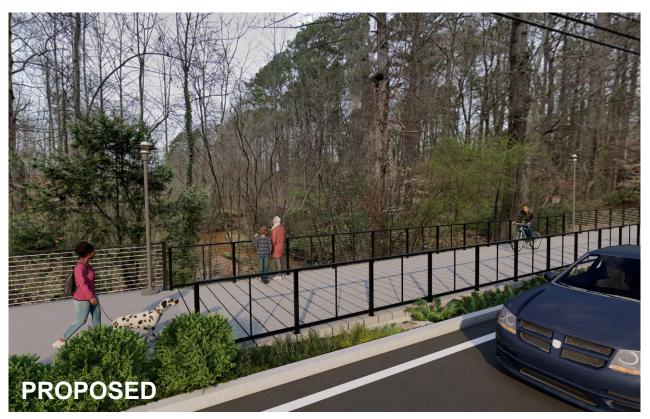
VIEW AT STA 86+50, LOOKING EAST











ROBERTS DRIVE SIDE PATH SCOPING REPORT

04.6 PERSPECTIVES

ROBERTS DRIVE AT ROSWELL ROAD (PHASE 2)

VIEW AT STA118+00, LOOKING EAST TOWARDS ROSWELL ROAD















04.7 STREAM CROSSING ALTERNATIVES

BOARDWALK / PEDESTRIAN BRIDGE

More expensive option

May add to the aesthetics of the side path

Least disruptive to stream bad



PermaTrak Concrete Boardwalk
PermaTrak



Pedestrian Bridge TrueNorth Steel

CULVERT EXTENSION AND GRADING

Least expensive option

More disruptive to stream bed

Would allow for continuous concrete path

May take longer to obtain permits



CULVERT OVER WATER BODY

Images are shown as examples only and do not represent final design.

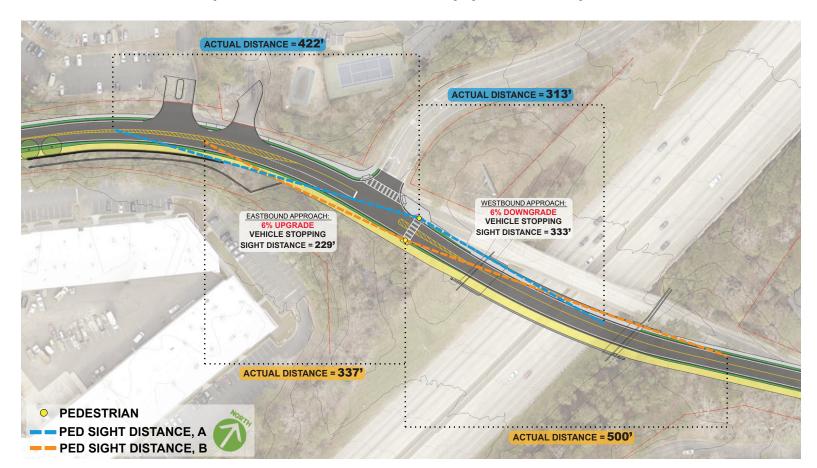
78

THIS PAGE LEFT INTENTIONALLY BLANK

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS 79

04.8 MID-BLOCK CROSSING SIGHT DISTANCES

CROSSING A (NEAR PRIDE PLACE) (PHASE 1)



Location: STA 12+50

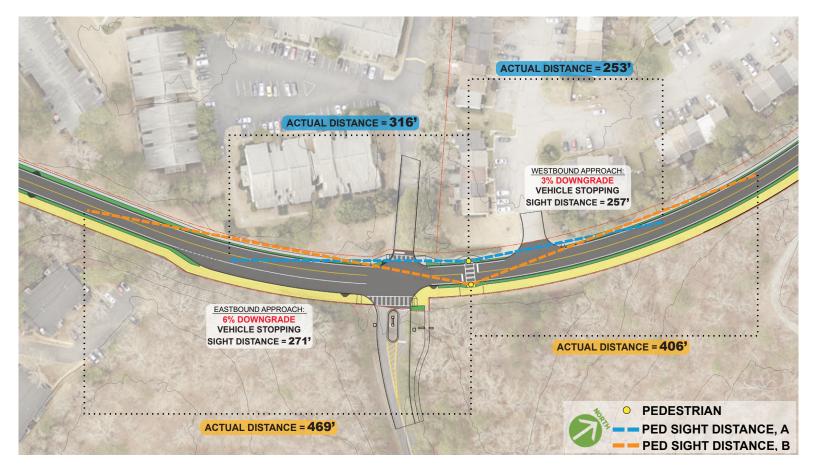
Roadway Speed Limit: 35 MPH

Design Speed: 40 MPH

Pedestrian Sight Distance Requirement: 694 FT

The sight distance study was limited to the ROW limits. The minimum pedestrian sight distance required by GDOT is 694 ft. Each of the pedestrian site limits do not meet this required amount. The vehicle stopping distance meets the requirements for vehicles traveling eastbound on Roberts Drive. However, due to the bridge wall, the vehicle stopping distance is not met for westbound traffic. A potential solution would be to move the mid-block crossing to the east side of Pride Place. Further study will need to be required to determine the solutions for the vehicle stopping distance and pedestrian sight distances required for this crossing.

CROSSING B (NEAR ISLAND FORD PARKWAY) (PHASE 1)



Location: STA 34+05

Roadway Speed Limit: 35 MPH

Design Speed: 40 MPH

Pedestrian Sight Distance Requirement: 563 FT

The sight distance study was limited to the ROW limits. The minimum pedestrian sight distance required by GDOT is 563 ft. Each of the pedestrian site limits do not meet this required amount. The vehicle stopping distance meets the requirements for vehicles traveling eastbound on Roberts Drive. However, the vehicle stopping distance is not met for westbound traffic. The required distance is only short by 4 ft., therefore if the crosswalk is shifted to the left, the required vehicle stopping distance can be met. Further study will need to be required to determine the solutions for the vehicle stopping distance and pedestrian sight distances required for this crossing.

04.8 MID-BLOCK CROSSING SIGHT DISTANCES

CROSSING C (NEAR SUMMER CROSSING) (PHASE 1)



Location: STA 52+44

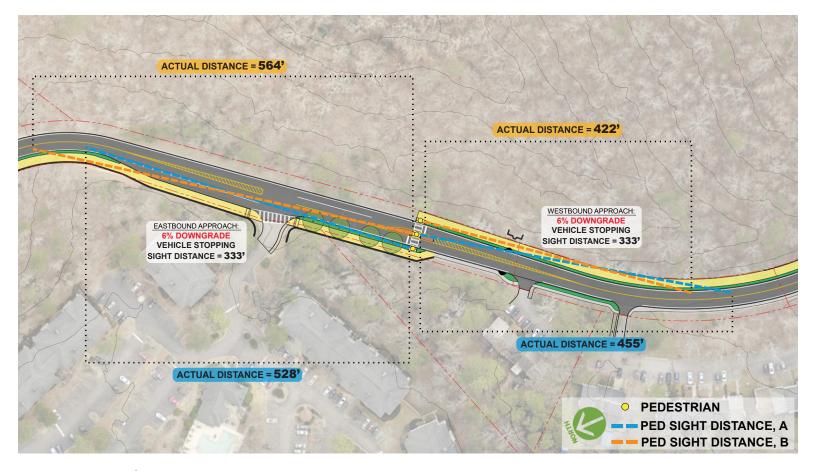
Roadway Speed Limit: 35 MPH

Design Speed: 40 MPH

Pedestrian Sight Distance Requirement: 402 FT

The sight distance study was limited to the ROW limits. The minimum pedestrian sight distance required by GDOT is 402 ft. Each of the pedestrian site limits meet this required amount. The vehicle stopping distance meets the requirements for vehicles traveling both eastbound and westbound along Roberts Drive. Further study is recommended to verify the vehicle stopping distances and pedestrian sight distances required for this crossing.

CROSSING C (NEAR SUMMER CROSSING) (PHASE 2)



Location: STA 52+44

Roadway Speed Limit: 35 MPH

Design Speed: 40 MPH

Pedestrian Sight Distance Requirement: 402 FT

The sight distance study was limited to the ROW limits. The minimum pedestrian sight distance required by GDOT is 402 ft. Each of the pedestrian site limits meet this required amount. The vehicle stopping distance meets the requirements for vehicles traveling both eastbound and westbound along Roberts Drive. Further study is recommended to verify the vehicle stopping distances and pedestrian sight distances required for this crossing.

04.8 MID-BLOCK CROSSING SIGHT DISTANCE

CROSSING D (NEAR LEXINGTON DRIVE) (PHASE 2)



Location: STA 52+44

Roadway Speed Limit: 35 MPH

Design Speed: 40 MPH

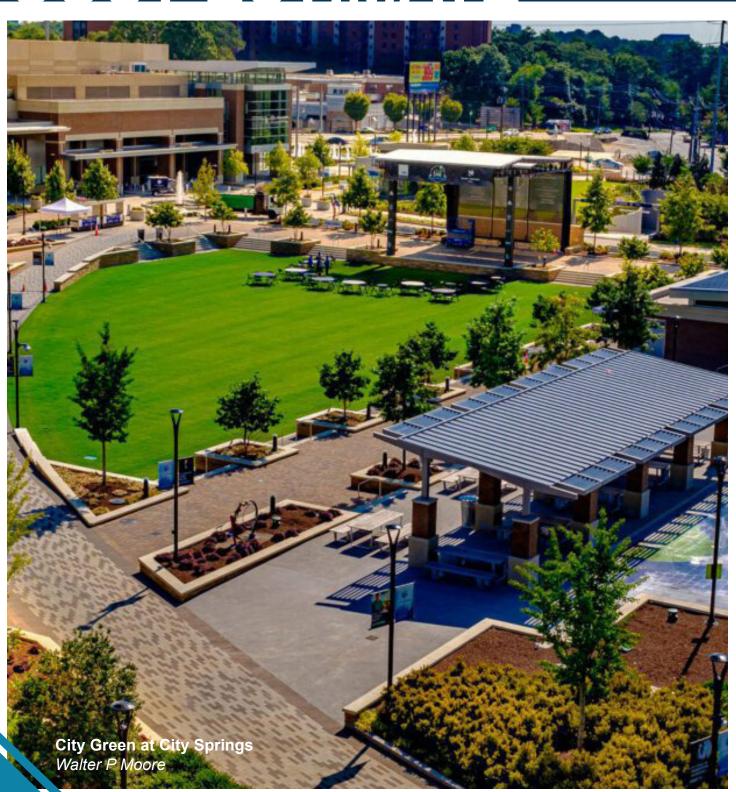
Pedestrian Sight Distance Requirement: 402 FT

The sight distance study was limited to the ROW limits. The minimum pedestrian sight distance required by GDOT is 402 ft. Each of the pedestrian site limits meet this required amount. The vehicle stopping distance meets the requirements for vehicles traveling both eastbound and westbound along Roberts Drive. Further study is recommended to verify the vehicle stopping distances and pedestrian sight distances required for this crossing.

THIS PAGE LEFT INTENTIONALLY BLANK

05 COST ESTIMATE





05.1 OVERALL COST ESTIMATE

05.2 FUNDING OPPORTUNITIES

PHASE ONE

DEMOLITION AND GRADING	645,004.50
EROSION AND SEDIMENT CONTROL	86,095.00
SIDE PATH AND WALLS	963,765.00
ROADWAY IMPROVEMENTS	186,916.00
UTILITY: WATER AND STORMWATER IMPROVEMENTS	7,125.00
AMENITIES	247,500.00
LANDSCAPE	106,205.00
_,	
SUBTOTAL:	2,242,610.50
SUBTOTAL:	2,242,610.50
SUBTOTAL: PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%)	2,242,610.50 560,652.63
SUBTOTAL: PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) CONTINGENCY (20%)	2,242,610.50 560,652.63 448,522.10 269,113.26
SUBTOTAL: PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) CONTINGENCY (20%) DESIGN AND ENGINEERING (12%)	2,242,610.50 560,652.63 448,522.10 269,113.26

PHASE TWO

DEMOLITION AND GRADING 1,521,882.50 EROSION AND SEDIMENT CONRTOL 98,900.00 SIDE PATH AND WALLS 3,732,105.00 ROADWAY IMPROVEMENTS 194,721.00 UTILITY: WATER AND STORMWATER IMPROVEMENTS 14,000.00 AMENITIES 303,875.00 LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00 TOTAL COST: \$11,661,246.40		
SIDE PATH AND WALLS 3,732,105.00 ROADWAY IMPROVEMENTS 194,721.00 UTILITY: WATER AND STORMWATER IMPROVEMENTS 14,000.00 AMENITIES 303,875.00 LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	DEMOLITION AND GRADING	1,521,882.50
ROADWAY IMPROVEMENTS 194,721.00 UTILITY: WATER AND STORMWATER IMPROVEMENTS 14,000.00 AMENITIES 303,875.00 LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	EROSION AND SEDIMENT CONRTOL	98,900.00
UTILITY: WATER AND STORMWATER IMPROVEMENTS 14,000.00 AMENITIES 303,875.00 LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	SIDE PATH AND WALLS	3,732,105.00
AMENITIES 303,875.00 LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	ROADWAY IMPROVEMENTS	194,721.00
LANDSCAPE 123,590.00 SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	UTILITY: WATER AND STORMWATER IMPROVEMENTS	14,000.00
SUBTOTAL: 5,989,073.50 PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) 1,497,268.38 CONTINGENCY (20%) 1,197,814.70 DESIGN AND ENGINEERING (12%) 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	AMENITIES	303,875.00
PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%) CONTINGENCY (20%) DESIGN AND ENGINEERING (12%) UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING RIGHT OF WAY ACQUISITION AND EASEMENTS 1,497,268.38 718,688.82 UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 886,901.00	LANDSCAPE	123,590.00
CONTINGENCY (20%) DESIGN AND ENGINEERING (12%) UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	SUBTOTAL:	5,989,073.50
DESIGN AND ENGINEERING (12%) UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	PROJECT MGMT / MOBILIZATION / BONDING & INSURANCE (25%)	1,497,268.38
UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHTING 1,371,500.00 RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	CONTINGENCY (20%)	1,197,814.70
RIGHT OF WAY ACQUISITION AND EASEMENTS 886,901.00	DESIGN AND ENGINEERING (12%)	718,688.82
	UTILITY: POWER RELOCATION AND PEDESTRIAN LIGHT	TING 1,371,500.00
TOTAL COST: \$ 11,661,246.40	RIGHT OF WAY ACQUISITION AND EASEMENTS	886,901.00
	TOTAL COST:	\$ 11,661,246.40

PROJECT TOTAL

PHASE ONE	4,848,293.49
PHASE TWO	11,661,246.40
TOTAL COST:	\$ 16,509,539.89

The cost estimate is based on the conceptual plan and is approximate cost only. The detailed cost estimate can be found in section 07.4 of the appendix.



Abernathy Greenway Park City of Sandy Springs

05.2 FUNDING OPPORTUNITIES

FEDERAL

- Transportation Alternatives Program (TAP)
- Rebuilding American Infrastructure with Sustainability and Equity (RAISE) Grants
- Federal Recreational Trails Program (RTP) Grants
- Congestion Mitigation and Air Quality Program (CMAQ)
- Federal Lands and Tribal Transportation Program (FLTTP)
- National Recreation Trails (NRT)
- Community Development Block Grant CDBG)
- Transit-Oriented Development (TOD) Pilot Program
- Highway Safety Improvement Program (HSIP)
- Advanced Transportation and Congestion Management Technologies Deployment (ATCMTD)
- Federal Lands Access Program (FLAP)
- Carbon Reduction Program (CRP)

STATE

- Georgia Department of Natural Resources (DNR) Grants
- GDOT Maintenance and Improvement Grant (LMIG)
- GDOT Transportation Enhancement (TE) Program
- Georgia Transportation Infrastructure Bank (GTIB)
- Quick Response projects
- GDOT Local Bridge Program (LBP) and Low-Impact Bridge Replacement Program (LIBRP)
- Safe Routes to School (SRTS)

REGIONAL

- Livable Centers Initiative (LCI)
- Community Development Assistance Program
- Transportation Improvement Program (TIP) (TIP is used to distribute some federal funding sources like CMAQ, TAP, and STBG, CRP)

SALES TAX

90

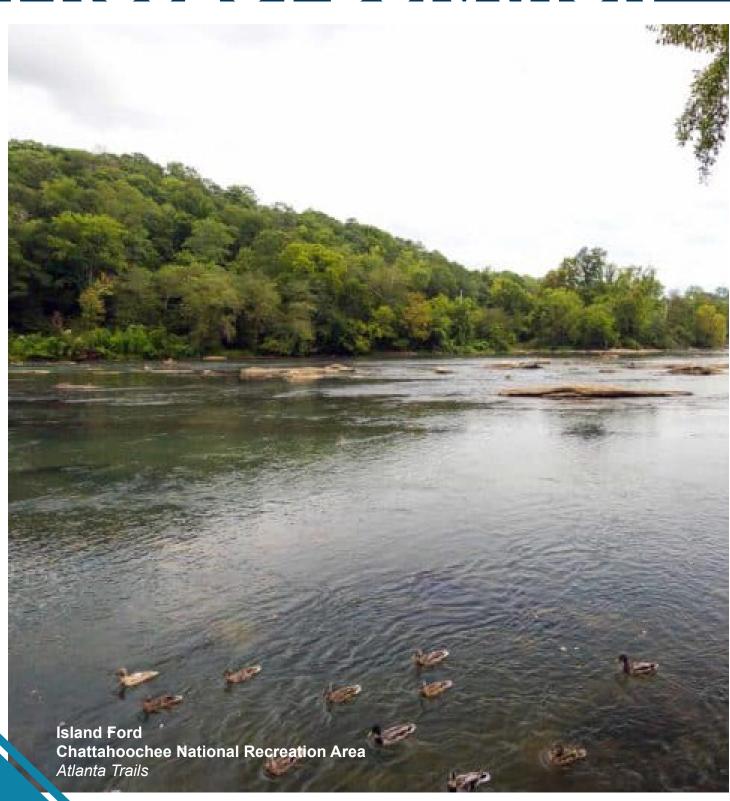
• Fulton County TSPLOST

THIS PAGE LEFT INTENTIONALLY BLANK.

ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS

06 PROJECT DURATION





06.1 TIMELINE



STUDY ACCEPTANCE

City Council Adopts Scoping Study

PROJECT PRIORITIZATION

Funding Identificiation Project Timeline Finalized

PRELIMINARY ENGINEERING

Design Public Input

FINAL DESIGN

Construction Plans Public Input ROW Acquisition

CONSTRUCTION

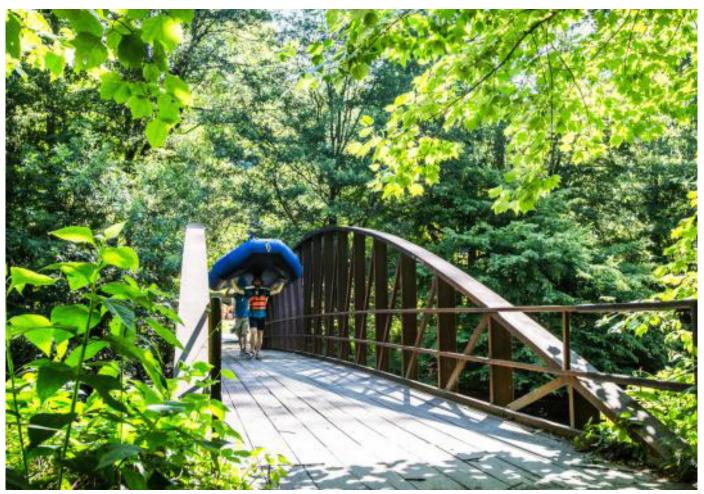
Bidding
Construction Phase 1
Construction Phase 2

PROJECT COMPLETED

Side Path Open to Public

This graphic shows a general timeline for the Roberts Drive side path.

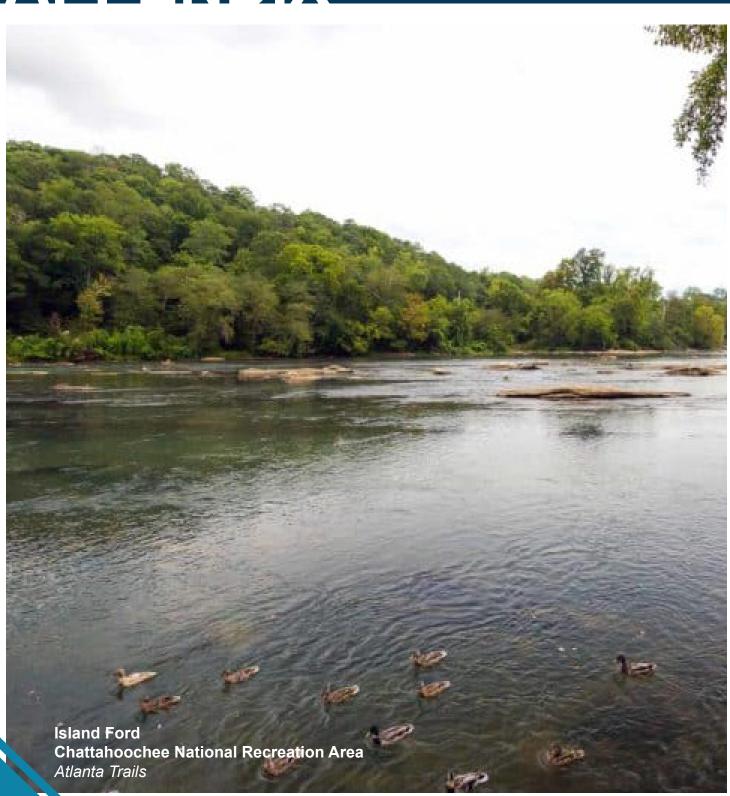
With the scoping study for the Roberts Drive side path complete, the next steps to be taken include project acceptance by city council, and funding source identification and allocation. As funding is obtained the project can enter final design and cost estimating. The project will be constructed in two phases as funding is available. The timeline for this project is expected to take 3 years from project acceptance to phase one open for public use, with phase two to follow. The project timeline can lengthen depending on funding availability. The chart to the right shows a general timeline for the project from study completion to project completed.



Island Ford, Chattahoochee National Recreation Area Visit Sandy Springs

07 APPENDIX



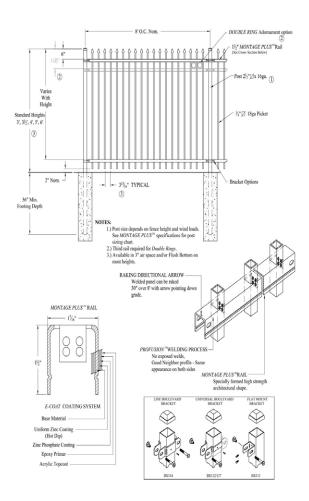


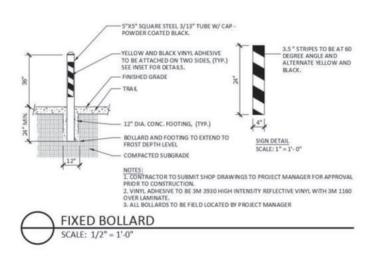
07.1	CONSTRUCTION DETAILS
07.2	REPORTS (ECOLOGICAL AND CULTURAL RESOURCES)
07.3	COORDINATION MEETINGS
07.4	COST ESTIMATE
07.5	PHASE TWO ALTERNATIVES COMPARISON
07.6	PHASE TWO ALTERNATIVE ALIGNMENT
07.7	PHASE 2, SHEET 2-5
07.8	RESOURCES

FENCING

98

BOLLARD





STEEL RAIL ON TOP OF 3"X1-1/2"X3" STEEL FLANGE T WITH SLOT TO RECIEVE STEEL ENSION CARLE RAILING PLATE FLANGE SEE CABLE TENSION FRONT ELEVATION VIEW 1/4" STEEL TUBE WELDED TO -ASSEMBLY LAU BOLT HAYDWARE TO BE STANLESS STEE STEEL PLATE TUBE SLEEVE ALL STEEL POSTS, NALS, AND STEEL PLATES TO HAVE A POVIDER COATED HINSH CONTRACTOR TO PROMISE - STEEL FLANGE WITH HOLE 1"X1" X 3/8" SQUARE RECEIVE LOCKABLE PADLO STEEL FLANGE TO RECEIVE PADLOCK 6"X6" X 3/8" SQUARE TO BE MOUNTED AND FIXE STEEL PLATE INTO CONCRETE STEEL TUBE SLEEVE, WELD POST REMOVABLE SMOOTH TO STEEL PLATE TO BE MOUNTED AND FIXED POST INTO CONCRETE STEEL BASE **ASSEMBLY** ON CONC. FLUME PLATE 8 HANDRAIL DETAIL N.T.S.

LIGHTING



1 Luminaire DMS60-55W48LED4K-R-ACDR-LE3A-VOLT-GN8TX

Description of Components

Finial: Decorative cast 356 aluminum, mechanically assembled.

Cupola: Decorative spun aluminum 1100-0, mechanically mounted on hood

Hood: Spun aluminum 1100-0 dome, mechanically assembled on the luminaire

Skirt: Spun 1100-0 aluminum, mechanically assembled on the luminaire.

Access-Mechanism: A die cast A360 aluminum technical ring with latch and hinge. The mechanism shall offer toolfree access to the inside of the luminaire. An embedded menory-retentive gasket shall ensure weatherproofing.

Globe: (ACDR), Made of one-piece seamless injected-moulded impact-resistant (DR) acrylic having an inner prismatic surface. The globe is mechanically assembled and sealed onto the lower part of the heat sink.

Lamp: (Included), Lamp type Philips Lumileds Luxeen R. Composed of 48 high-performance white LEDs, 55w lamp wattage, Color temperature of 4000 Kelvin nominal, 70 CRI. Operating lifespan based on TM21 extrapolation to get results after which 50% of LEDs still emits over 70% (L70) of its original tumen output. Use of metal core board insurse greater tartarsfer and longer lifespan of the light engine. The LED circuit board is included with a connector, (no connection wire

A - MID BLOCK LED Standard w_Finial

27 1/2"

LUMEC



EPA: 1.42 sq ft / weight: 42 lb (19.1 kg) Note: 30 image may not represent color or call. Logos above include link, click to acces

1 Luminaire DMS50-70W64LED4K-R-LE3S-VOLT-GN8TX

Description of Components

Hood: A die cast A360.1 aluminum dome complete with a cast-in technical ring with latch and hinge. The mechanism shall offer toolfree access to the inside of the luminaire. An embedded memory-retentive gasket shall ensure weatherproofing.

Housing: In a round shape, this housing is made of cast 356 aluminum, c/w a watertight grommet, mechanically assembled to the bracket with four boits 3/8-16 UNC. This suspension system permits for a full rotation of the luminaire in 90 degree

Light Engine: LEDgine composed of 5 main components: Lens / LED lamp / Optical System / Heat Sink / Driver Electrical components are RoHS compliant.

Lens: Made of soda-lime clear tempered glass curved lens, mechanically assembled and sealed onto the lower part of the heat sink.

Lamp: (Included), Lamp type Philips Lumifeds Luxeon R. Composed of 64 high-performance white LEDs, 70w lamp wattage. Color temperature of 4000 Kelvin nominal, 70 CRI. Operating lifespan based on TM21 extrapolation to get results after which 50% of LEDs still emits over 70% (L70) of its original lumen output. Use of metal core board insures greater heat transfer and longer lifespan of the light engine. The LED circuit board is included with a connector, (no connection wire

Optical System: (LE3S), IES type III (asymmetrical). Composed of high-performance optical grade PMMA acrylic refractor lenses to achieve desired distribution optimized to get maximum spacing, target lumen and a perfect lighting uniformity. Optical system is rated IP66, Performance shall be tested per LM63 and LM79 and TM15 (IESNA) certifying its photometric performance. Street side indicated.

PHILIPS LUMEC





Qty 1 Pole APR4U-13-LBC1-GN8TX

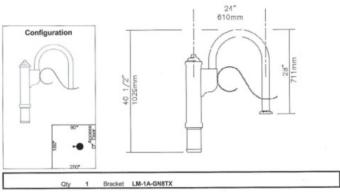
Pole Shaft: Shall be made from a 4" (102mm) round extruded 6061-T6 aluminum tubing, having a 0.228" (5.7mm) wall thickness, welded to both the bottom and top of the anchor plate.

Maintenance Opening: The pole shall have a 2" x 4 1/2" (51mm x 114mm) maintenance opening centered 20* (508mm) from the bottom of the anchor plate, complete with a weatherproof aluminum cover and a copper ground lug.

Base Cover: Two piece round base cover made from cast 356 aluminum, mechanically fastened with stainless steel screws.

Note: A tenon will be provided when the luminaire or bracket does not fit directly on

IMPORTANT: Philips Lumec strongly recommends the installation of the complete lighting assembly with all of its accessories upon the anchoring of the pole. This will ensure that the structural integrity of the product is maintained throughout its lifetime.



Arm: Shall be made from bent 6061-T6 aluminum tubing, 2 3/8" (60mm) outside diameter, welded

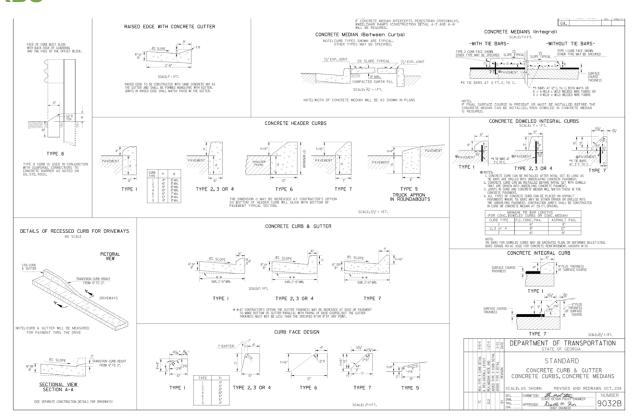
Decorative Element: Scroll made of bent aluminium, welded to the arm.

Central Adaptor: Made of aluminum 6061-T6, 5" (127mm) outside diameter. Complete with a cast 356 aluminum fitter that slip-fits 9" (229mm) over a 4" (102mm) outside diameter pole tenon. Mechanically assembled using two sets of four set screws at 90 degrees around the bracket.

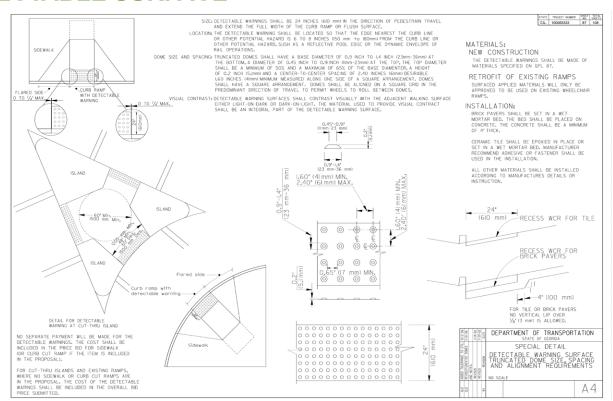
ROBERTS DRIVE SIDE PATH SCOPING REPORT **CITY OF SANDY SPRINGS** 99

CURBS

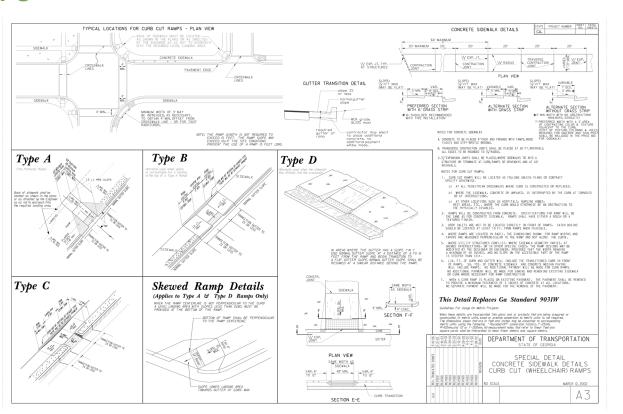
100



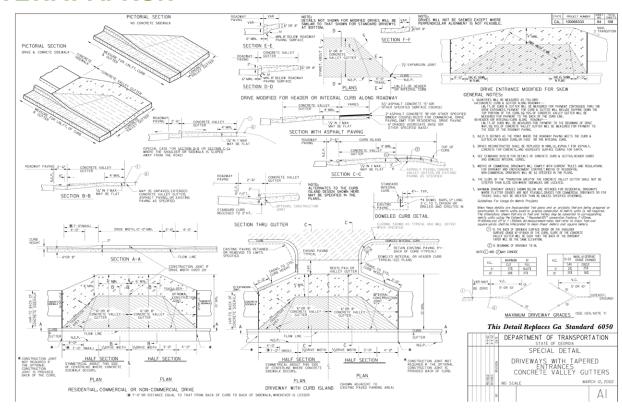
DETECTABLE SURFACE



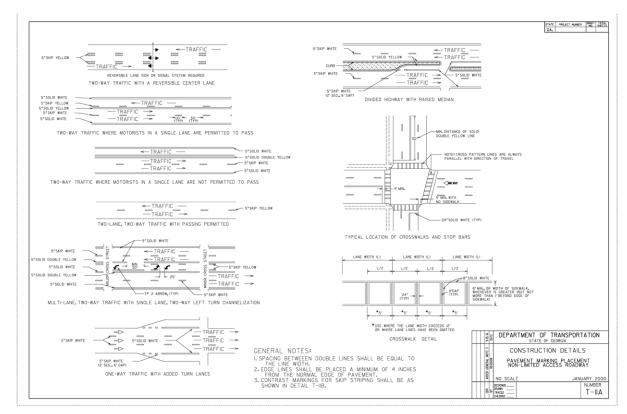
RAMPS



DRIVEWAY APRON

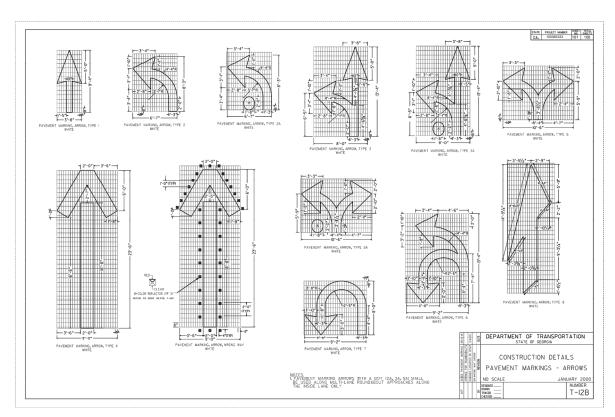


PAVEMENT MARKING

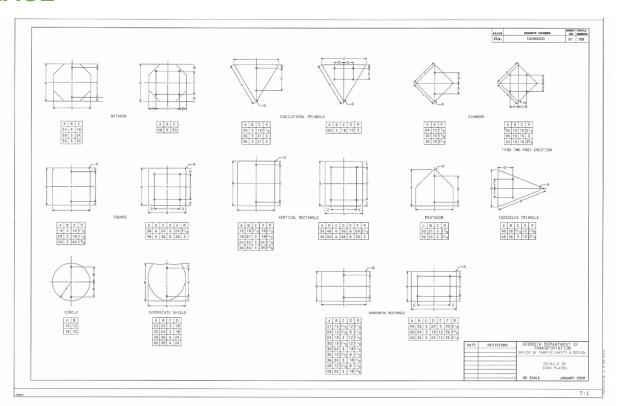


PAVEMENT MARKINGS

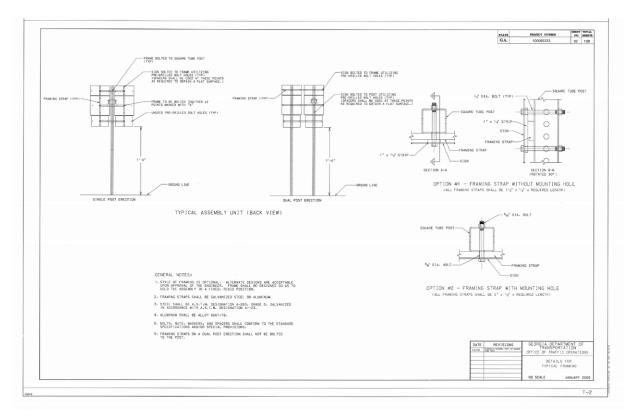
102



SIGNAGE

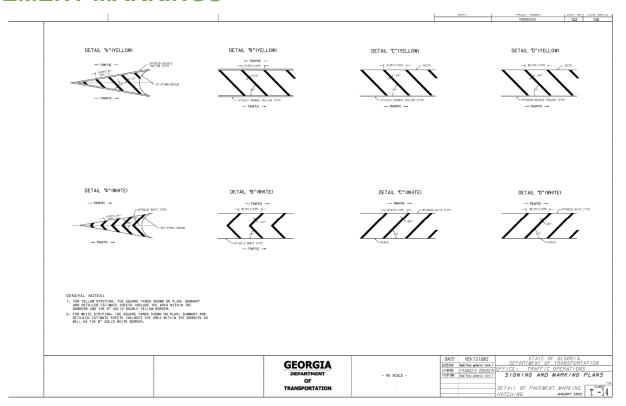


SIGNAGE

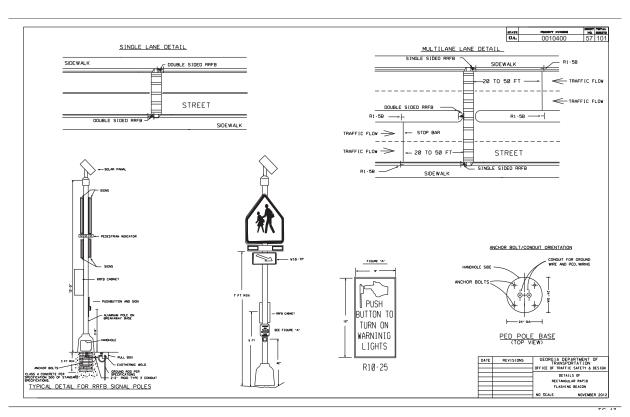


103

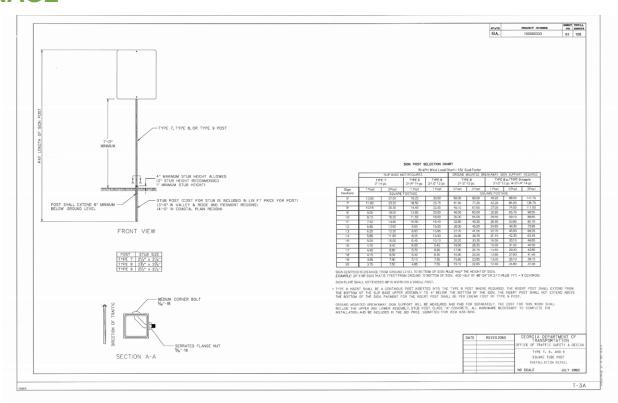
PAVEMENT MARKINGS



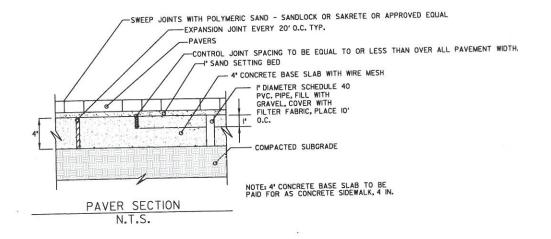
RAPID RECTANGULAR FLASHING BEACON



SIGNAGE



BRICK PAVER



POURED IN PLACE WALL

NOTES: 1. SEE RETAINING WALL ENVELOPE PLANS FOR PLAN AND ELEVATION VIEW OF WALLS. 2. REFER TO NOTES ON SHEET 40-02 FOR ADDITIONAL REQUIREMENTS AND GDOT PRW-1 - TYPE P2 DETAIL. 2. STONE TO BE LAID IN ASHLAR PATTERN. WALLS NO. 1, 2, AND 3 TO HAVE THE SAME STONE VENEER MATERIAL AND PATTERN. 3. ALL STATIONS AND OFFSETS ARE FROM THE FACE OF GRANITE STONE VENEER. FACE OF WALL IS FACE OF GRANITE STONE VENEER. VENEER. RAILING, 2" ROUND SOLID STEEL BLACK POWDER COAT FINISH —4" THICK GRANITE CAP TO MATCH GRANITE CAP. GRANITE CAP TO VENEER STONE. CAP STONES TO MATCH VENEER STONE. CAP STONES TO BE MIN. 18" IN LENGTH BE MIN. 18" IN LENGTH FRONT FACE OF WALL-CAP AND RAILING MORTAR JOINT AT BACK FACE OF WALL FACE OF WALL ELBERTON GRANITE STONE, ASHLAR PATTERN, STONE MASONRY LAID IN - FLBERTON GRANITE STONE ASHLAR PATTERN 6" THICK CONC. SIDEWAL STONE, ASHLAR PATTERN, STONE MASONRY LAID IN STIFF HIGH STRENGTH MORTAR, REFER TO SPEC. STIFF HIGH STRENGTH MORTAR, REFER TO SPEC CONCRETE LEDGE-FOR STONE FACING FRONT FACE OF WALL -PROPOSED FINISHED GRADE REFER TO GDOT-PRW-1 - TYPE P2 DETAIL FINISHED GRADE AT BACK FACE OF WALL AT FRONT FACE OF WALL 6" THICK CONC. SIDEWALK - 4" EXPANSION JOINT WALL NO. 1 - PRW-1 - TYPE P2 WITH PARAPET / RAILING WALL NO. 3 - PRW-1 - TYPE P2 WITHOUT PARAPET $\fbox{2}$ TYPICAL RETAINING WALL NO. 1 AND NO. 3 SECTION N.T.S.

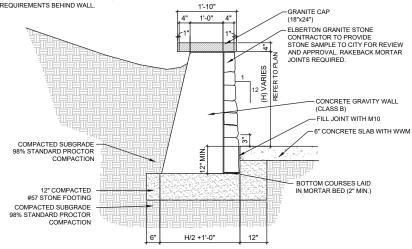
GRAVITY WALL

- NOTES:

 1. CONTRACTOR TO PROVIDE STONE VENEER, STONE CAPS & MORTAR COLOR SAMPLES FOR APPROVAL BY LANDSCAPE ARCHITECT OR CITY REPRESENTATIVE PRIOR TO ORDERING.

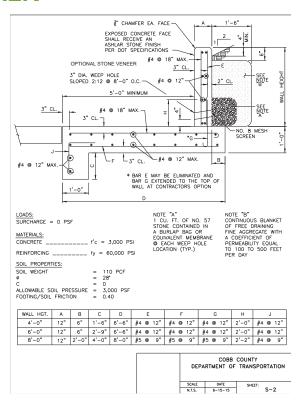
 2. STONE TO BE LAID IN ASHLAR PATTERN. WALLS NO. 1, 2, AND 3 TO HAVE THE SAME STONE VENEER MATERIAL AND PATTERN.

 3. REFER TO GDOT 9031L TYPICAL SECTION 'A' FOR DRAIMAGE BEGI IDEMENTS BEHIND WALL
- DRAINAGE REQUIREMENTS BEHIND WALL

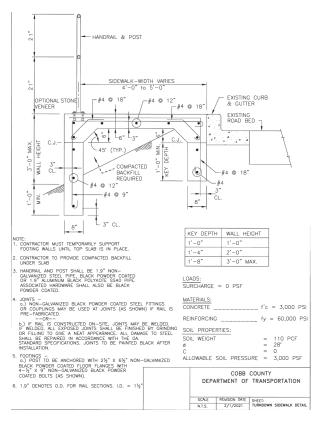


RUBBLE GRAVITY WALL DETAIL 40-03 N.T.S.

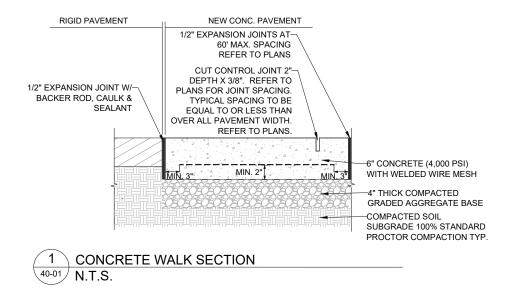
TURN UP SIDEWALK



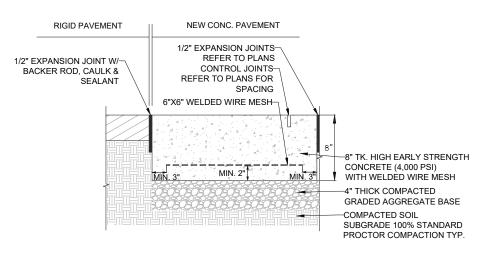
TURN DOWN SIDEWALK



CONCRETE WALK, 4IN

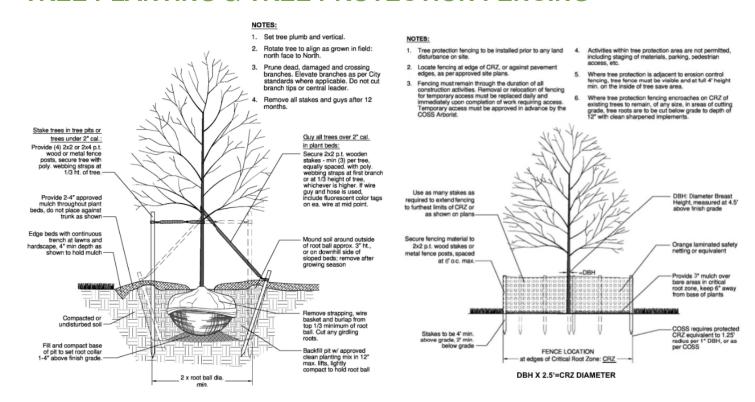


CONCRETE WALK, 6IN

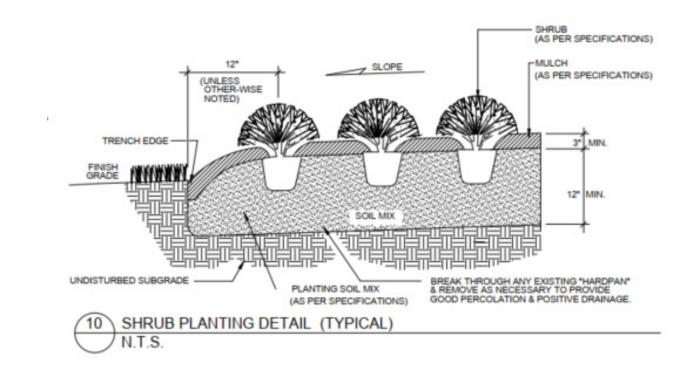


2 8" CONCRETE DRIVEWAY PAVING SECTION N.T.S.

TREE PLANTING & TREE PROTECTION FENCING



SHRUB PLANTING







Memo

Subject:

To:	The City of Sandy Springs		
From:	Rebecca Ricks	Email:	rebecca.ricks@atkinsglobal.com
Date:	26 April 2023	Ref:	Roberts Drive Sidepath; Atkins Job Number: 100084225

The proposed project would construct a pedestrian path alongside Roberts Road near Sandy Springs in Fulton County, Georgia. The project would begin near the intersection of Roberts Drive and Roswell Road and ends near the intersection of Roberts Drive and Pride Place (Figure 1). As part of the project planning process, a desktop review was conducted to identify potential State and Federal Waters within the proposed project limits. To conduct this desktop review, the following resources were utilized: USGS topographic maps, aerial imagery, National Wetland Inventory (NWI) maps, Federal Emergency Management Agency (FEMA) Floodplain Maps, and soil maps. Additionally, Google Earth Streetview was utilized to visualize approximate conditions on the ground. No field visits have been conducted as part of this desktop review. Based on the available data, there are two streams, one open water, and one additional buffer anticipated to be present within the project limits.

Although the Chattahoochee River is not located within the project limits, it is considered a trout stream and would be subject to a 50' state-mandated stream buffer. All other channels with perennial or intermittent flow and open waters within Fulton County are not considered trout waters and are subject to a 25' state-mandated stream buffer. Channels with ephemeral flow are also not considered trout waters and are not subject to state-mandated buffers in Fulton County.

The attached figures depict the approximate location of waters within the project limits based on the desktop review. These depictions should be used for planning purposes only. Please note that there may be additional resources present on the site that can only be detected with a field review. A field survey is required to determine the boundary and classification of any waters that may be present within the project limits.

Location 1: Buffer of Chattahoochee River

Ecology Desktop Review

- Based on the available mapping information, the Chattahoochee River is not located within the project limits, but the 50' state mandated stream buffer is within the project limits.
- Location 2: Stream near 9679 Roberts Drive
 - One stream crossing under Roberts Drive and one open water. Based on Google Streetview, it appears that the stream has been dammed on the south side of Roberts Drive to create an in-line pond.
 - A 25' state-mandated stream buffer would apply
 - Resource is a tributary of the Chattahoochee River





- · Location 3: Stream near 9500 Roberts Drive
 - o One stream crossing under Roberts Drive
 - A 25' state-mandated stream buffer would apply
 - Resource is a tributary of the Chattahoochee River

List of Attachments

- Streetview Images
- Figure 1 Project Vicinity Map
- Figure 2 Topographic Map
- Figure 3 Aerial Map
- Figure 4 NWI Map
- Figure 5 FEMA Map
- Figure 6 Soils Map

1



Location 2: View of south side of Roberts Drive near 9679 Roberts Drive. Based on this image, a stream has been dammed to create an in-line



Location 2: View of north side of Roberts Drive near 9679 Roberts Drive. Screenshot from Google Earth Streetview, image dated 3/2022.



Location 3: View of south side of Roberts Drive near 9500 Roberts Drive. Screenshot from Google Earth Streetview, image dated 3/2022.



Location 3: View of north side of Roberts Drive near 9500 Roberts Drive. Screenshot from Google Earth Streetview, image dated 3/2022.

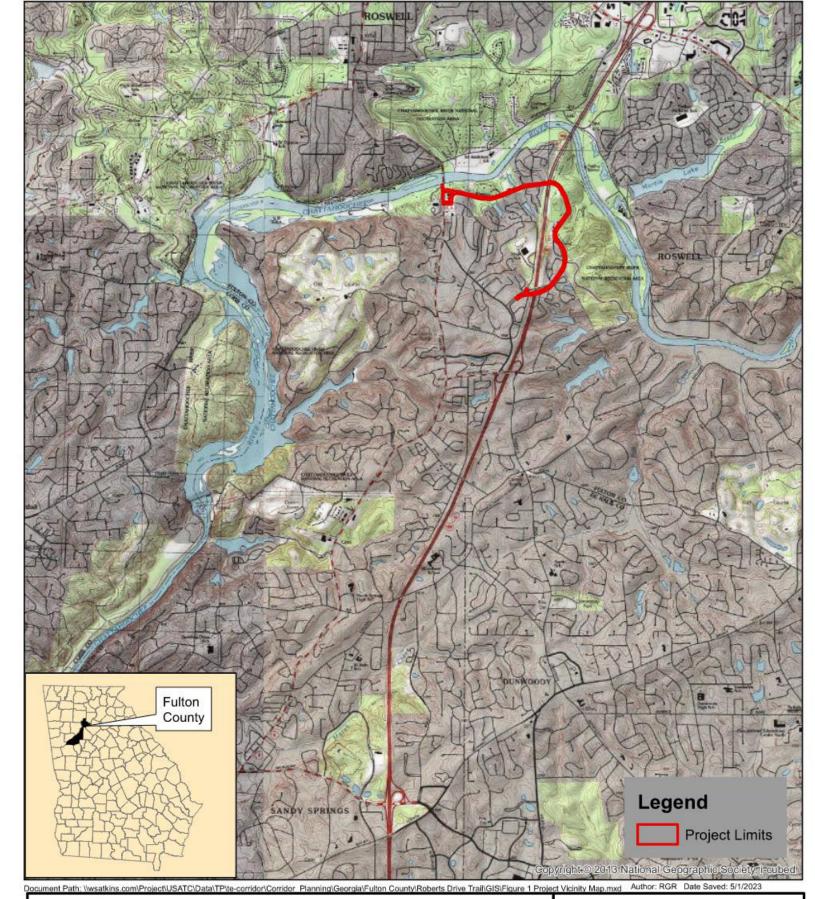
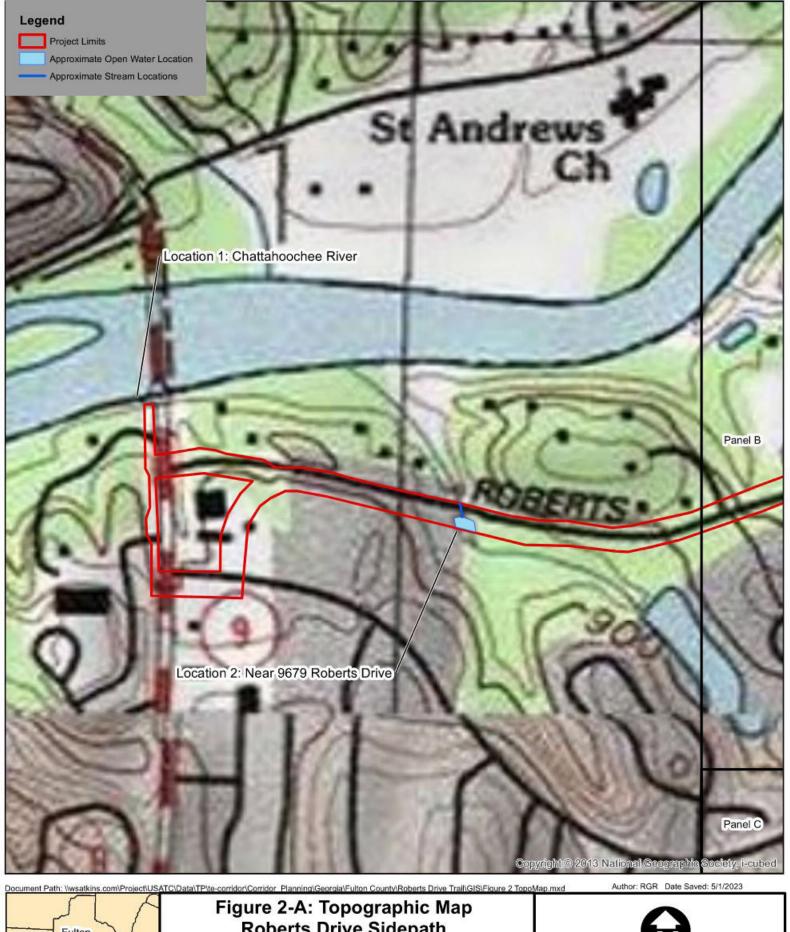
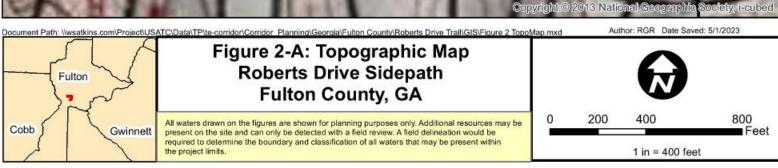


Figure 1: Vicinity Map Roberts Drive Sidepath

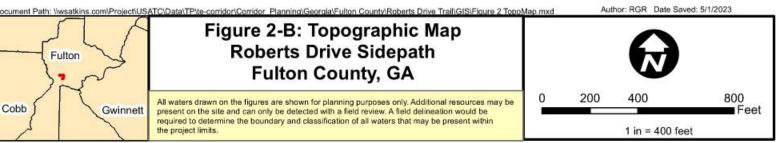
1.5 Miles 0.375 0.75

Fulton County, GA 1 in = 0.75 miles

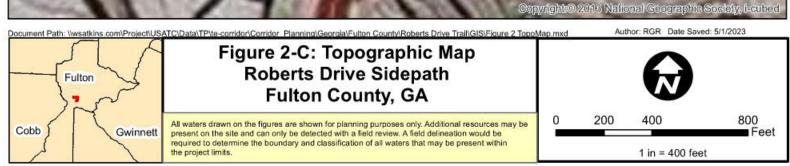


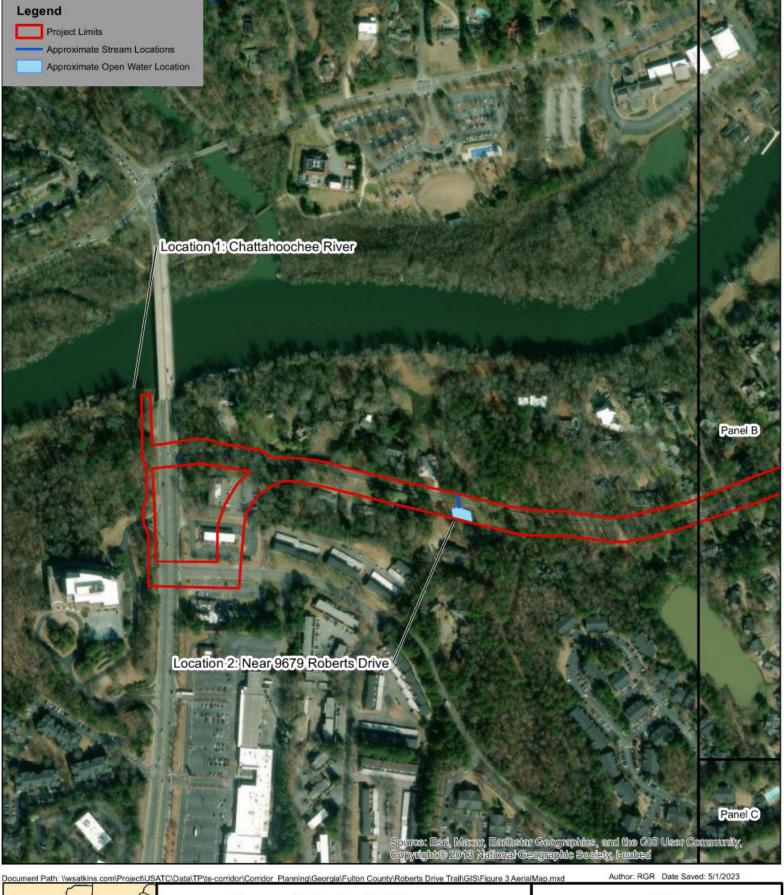


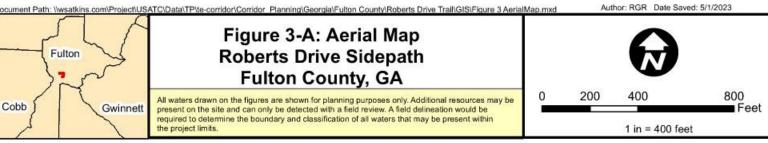


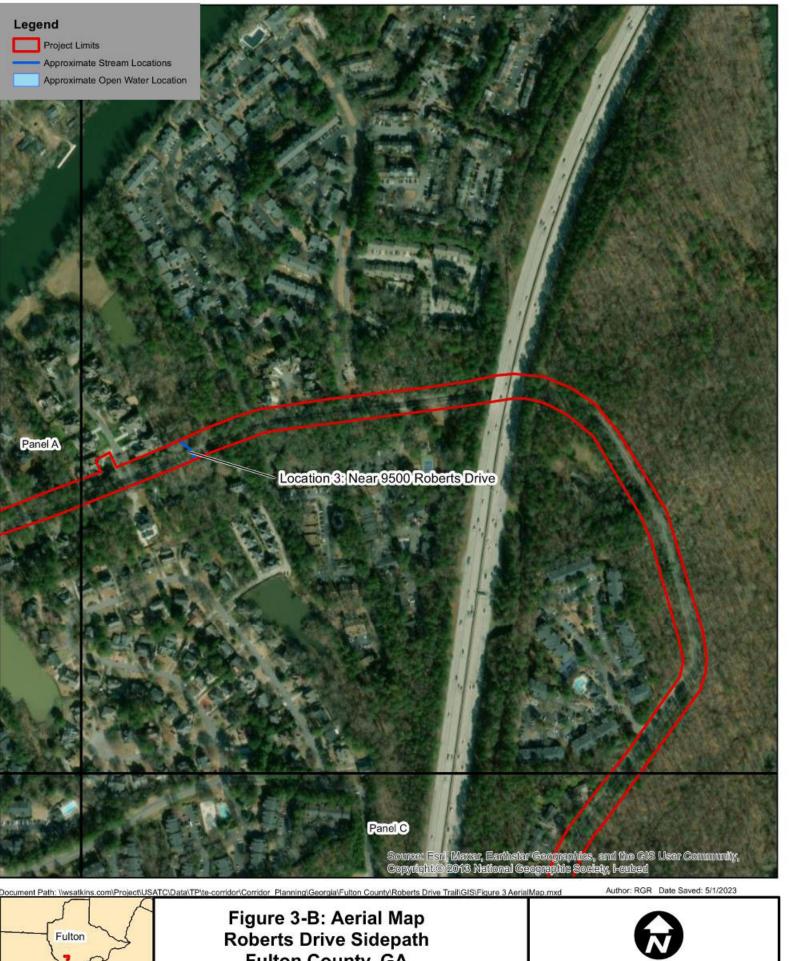


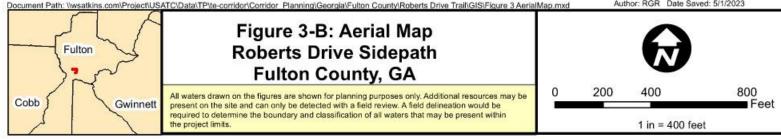




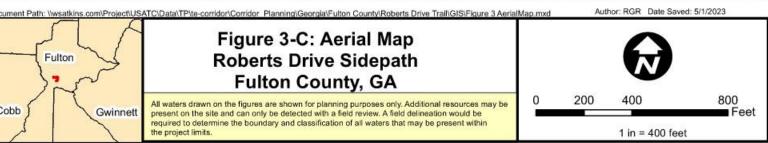


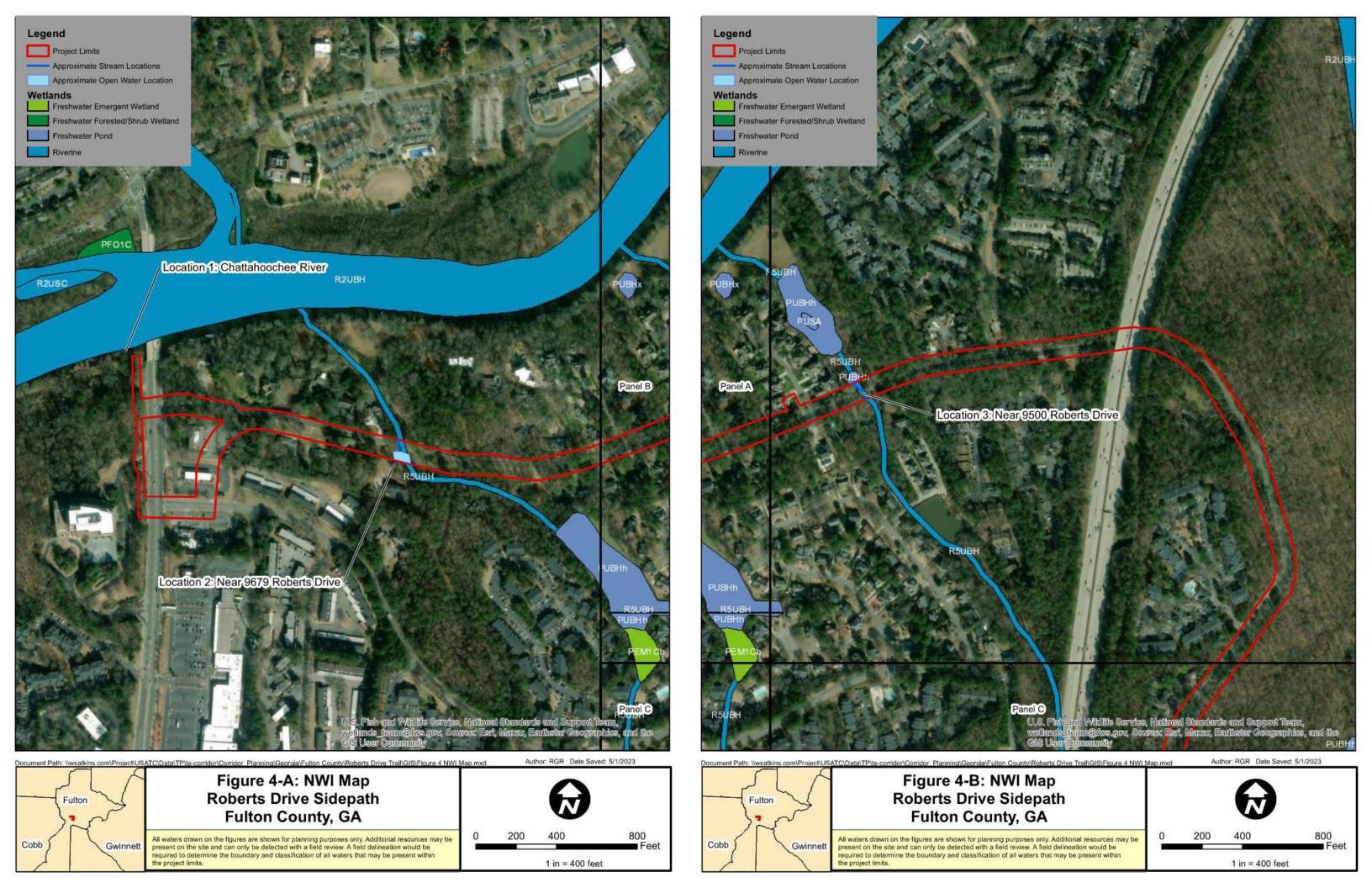


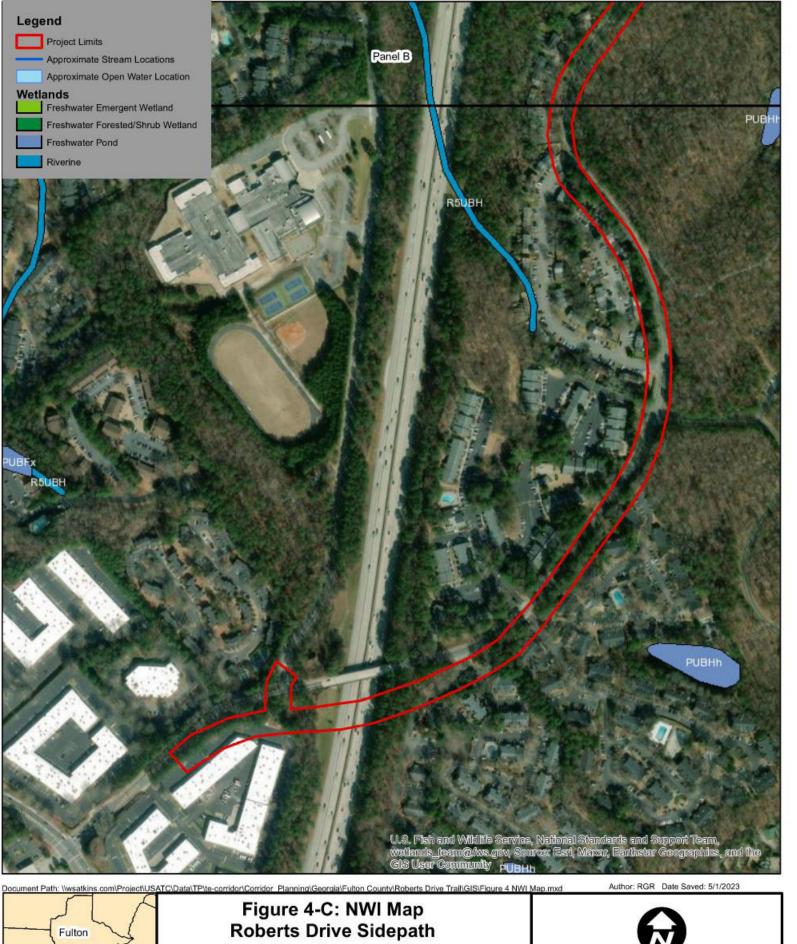


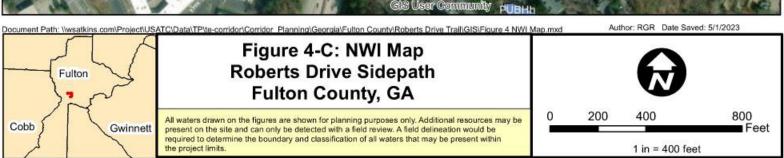


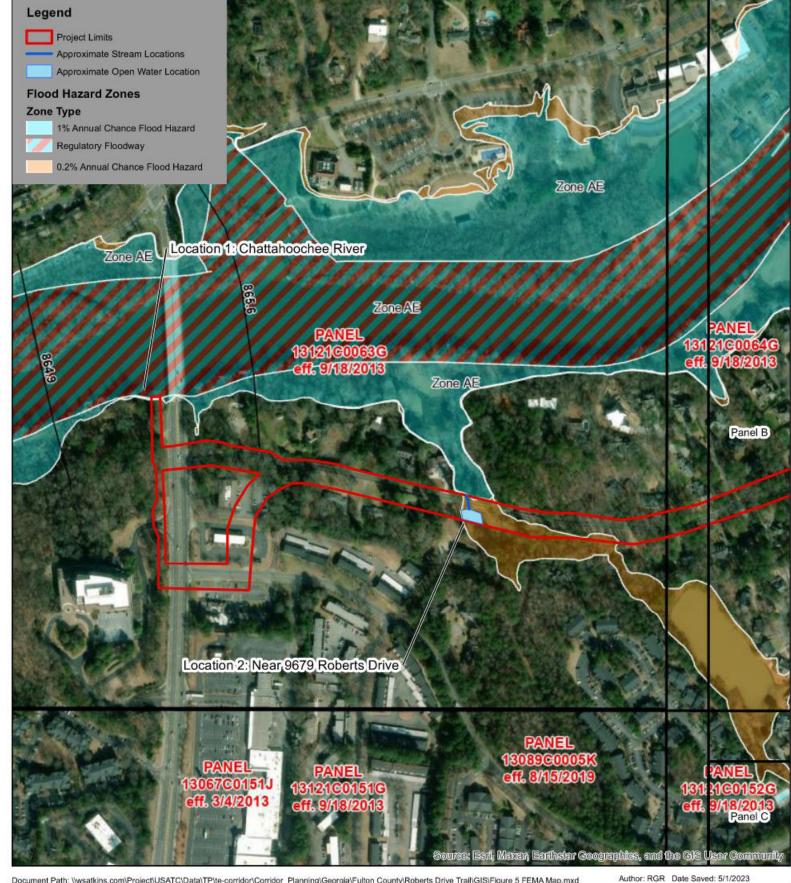


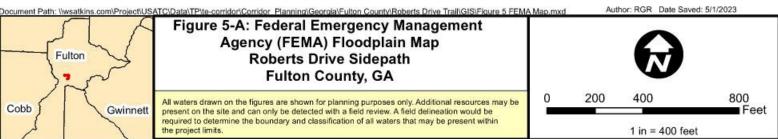


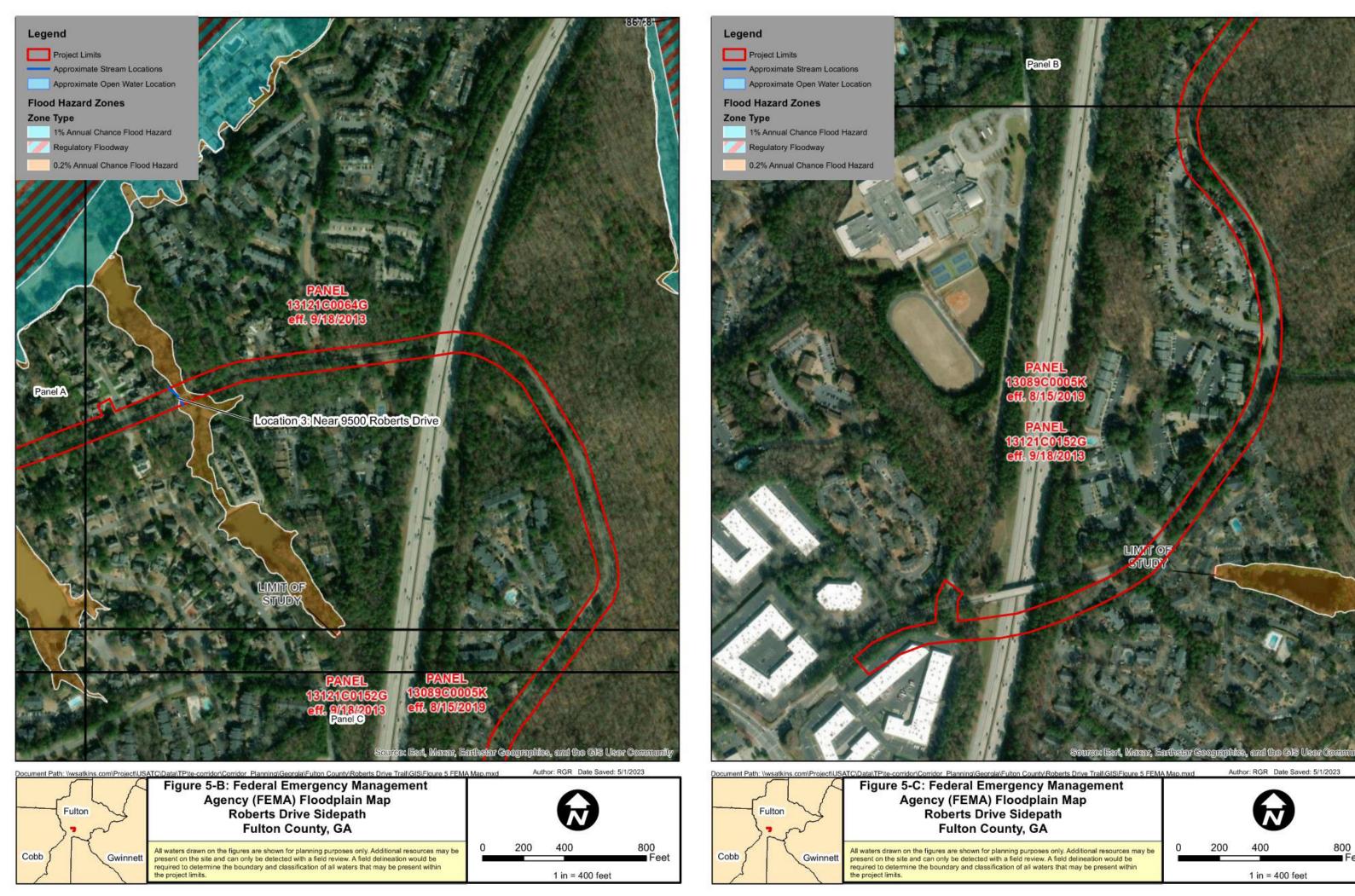


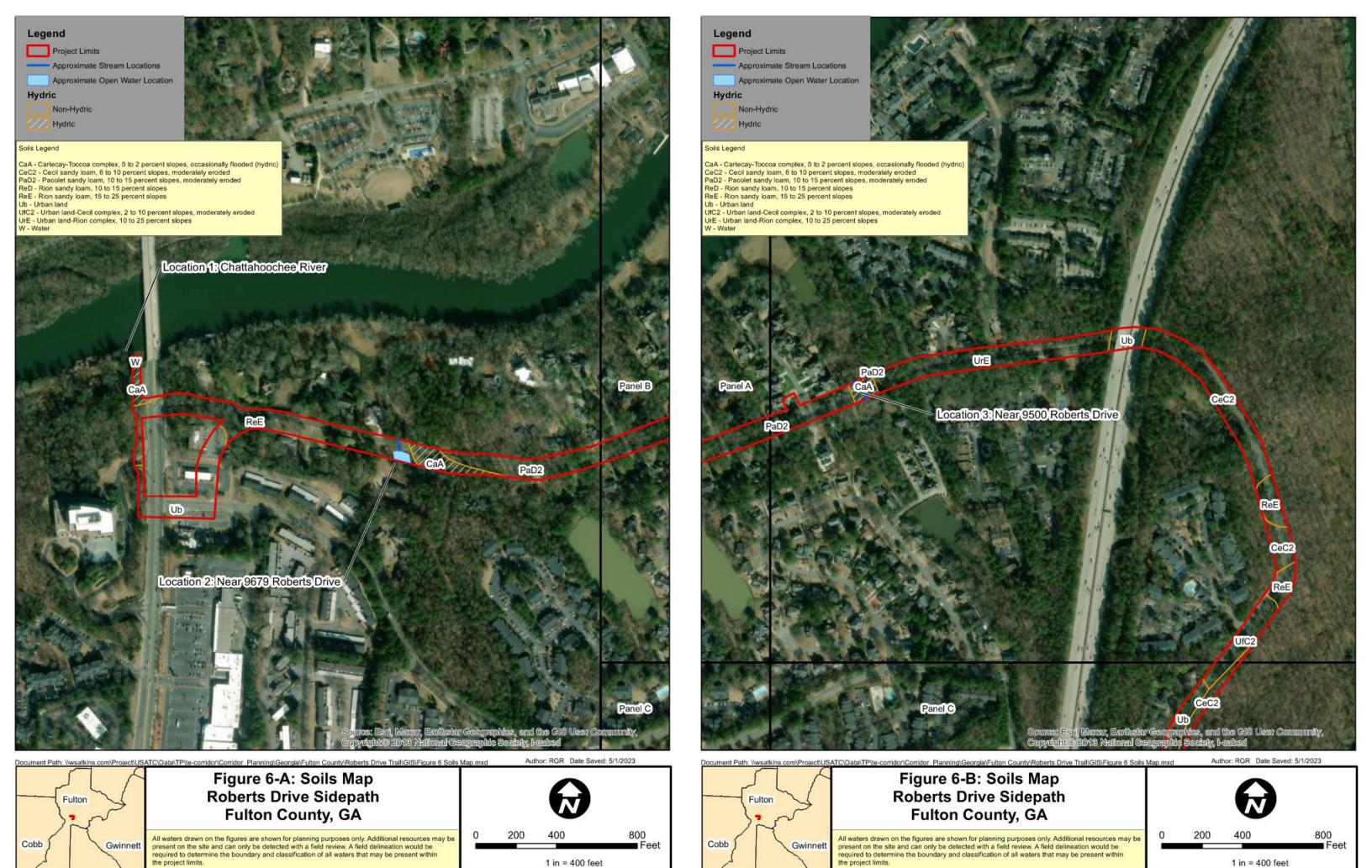


















Memo

To:	Files		
From:	Julia Templeton, Historian Brandy Norton, Archaeologist	Email:	Julia.templeton@atkinsglobal.com Brandy.norton@atkinsglobal.com
Phone:	678-247-2693	Date:	May 15, 2023
Subject:	S2123-1 Roberts Drive Sidepatl Resources Desktop Screening	h T0071 No	rth End Blvd Safety Study-Cultural

Project Description:

The goal of this project is to create a safer and more comfortable walking and bicycling environment along Roberts Drive as outlined in the city of Sandy Springs Next Ten Comprehensive Plan. The main project component would include a 12-foot wide sidepath with a 5-foot landscape buffer along Roberts Drive from Roswell Road to Dunwoody Place. Multiple alignments may be considered with up to 3 mid-block crossings anticipated to be used with location of the sidepath. Proposed locations of the mid-block crossings would include a stopping sight-distance check to ensure safe pedestrian/bicycle crossings on Roberts Drive.

Survey Purpose and Methodology:

The purpose of this memo is to conduct a desktop screening of the project area to identify cultural resources along the project corridor that could potentially be impacted by this proposed project. The area of potential effects (APE) is the geographic area or areas within which an undertaking may directly or indirectly cause changes in the character or use of cultural resources if any exist. The APE for this environmental screening consists of the properties fronting Roberts Drive from Roswell Road to Dunwoody Place and a segment of North River Parkway from Roswell Road to Winding River Drive.

Existing information on previously identified cultural resources was consulted to determine if any are located within the APE of this proposed streetscape improvement project. The review of existing information included the National Register of Historic Places (NRHP), National Historic Landmarks, and historic and archaeological properties included in Georgia's Natural, Archaeological, and Historic Resources Geographic Information System (GNAHRGIS) database. Desktop review of previously unidentified cultural resources was conducted using the Fulton County tax assessor's records, aerial photography and Google Streetview. Previous cultural resource surveys conducted of the project area were consulted as well.

Survey Results:

Based on the review of the above referenced sources, one NRHP listed resource is located within the proposed project's APE. This resource, the Isaac Roberts House, is also identified with GNAHRGIS ID numbers 33119 and 249569. This resource, located at 9725 Roberts Drive, was built in 1894 and listed on the NRHP in 2008 (Figure 1).

Fourteen additional resources 50 years of age or older are located within the proposed project's APE. These resources were identified via the Fulton County tax assessor's records. The following table provides the date of construction and location for each of these resources.

1

Date of Construction	Location	Resource Type
1963	9755 Roberts Drive	Ranch House
1972	9710 Roberts Drive	Ranch House
1970	320 Winding River Road	Condominium
1972	8975 Roswell Road	Commercial
1965	9705 Roberts Drive	Ranch House
1972	9680 Roberts Drive	Ranch House
1972	9695 Roberts Drive	Gabled Ell House
1972	9670 Roberts Drive	I-House
1963	9673 Roberts Drive	Split Level House
1968	9643 Roberts Drive	Ranch House
1972	9625 Roberts Drive	Ranch House
1963	9615 Roberts Drive	Ranch House
1973	9401 Roberts Drive	Apartment
1972	8601 Roberts Drive	Apartment

Features that potentially contribute to the eligibility of the Isaac Roberts House are located within and outside the existing right-of-way (ROW) of Roberts Drive. These features include trees, vegetation, fence, brick pavers, and stone pillars (see attached photographs). Once it has been determined which of these contribute to the significance of the historic property, the project design may need to take these features into consideration.

No NRHP eligibility recommendations have been made at this time. A field survey for cultural resources and subsequent report preparation would be necessary to evaluate the NRHP eligibility of any resources 50 years of age or older not already listed in the NRHP.

Three previous archaeological surveys intersect with the APE (Figure 1). A Phase I Archaeological Resources survey was conducted by the National Park Service (NPS) in 1979 which included the area surrounding the majority of Roberts Drive within the APE. No archaeological resources were recorded within the APE. A Phase I Archaeological Resources Survey was conducted by the Georgia Department of Transportation (GDOT) in 1997. This survey included a small area where Roberts Drive intersects with Roswell Road. Site 9FU284 was recorded during this survey (see description below). In 2018, GDOT conducted a Phase I Archaeological Survey that included the area where Roberts Drive Intersects with State Route (SR) 400. This survey revisited Sites 9FU753 and 9FU754 which were initially recorded by the Southeast Archaeological Center (SEAC) in 2009. The SEAC survey boundaries were not able to be located.

Four archaeological resources were identified within the proposed project's APE (see Figure 1 and table below). The Georgia Archaeological Site File (GASF) form for Site 9FU64 contained little information and the site's eligibility for the NRHP is unknown. No survey is associated with this site. It appears to have been recorded by an amateur archaeologist. The site should be avoided by design if possible. If not, further archaeological testing would be needed to determine site eligibility within the APE. Site 9FU284 is an historic artifact scatter that was determined to be ineligible for the NRHP. No considerations for this site need to be taken into account for design. No further archaeological work is recommended for this site. Site 9FU753 is a site with Civil War defense features which was determined to be eligible for the NRHP. This site is located on NPS property and does not extend into the existing ROW. This site should be avoided in the project design. Finally, Site 9FU754 is a mining site with unknown eligibility for the NRHP. The site should be avoided by design if possible. If not, further archaeological testing would be needed to determine site eligibility within the APE.

Site Number	Site Type	NRHP Eligibility
9FU64	Unknown	Unknown
9FU284	Historic Artifact Scatter	Ineligible
9FU753	Civil War Site	Eligible
9FU754	Mining Site	Unknown

2

Potential Contributing Features of the NRHP-Listed Isaac Roberts House

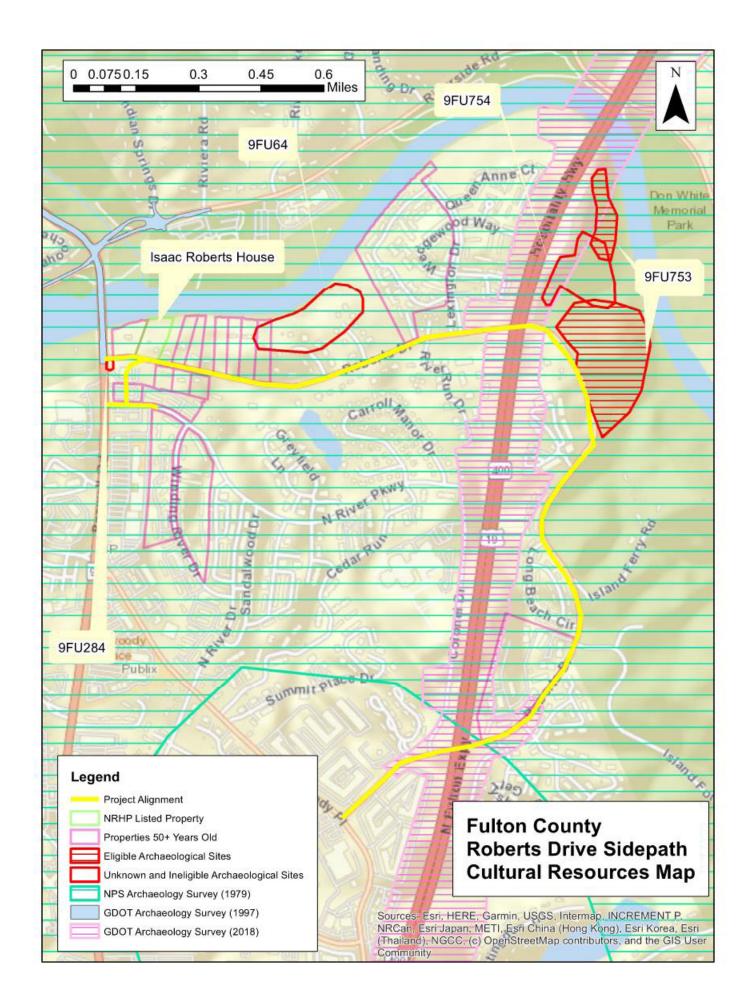


Photo 1: Stone pillar, brick pavers, and wood fence. View looking east from Roberts Drive.
(Source: Google Streetview)



Photo 2: Stone pillar, brick pavers, and vegetation. View looking west from Roberts Drive.

(Source: Google Streetview)



THIS PAGE LEFT INTENTIONALLY BLANK.

COORDINATION MEETINGS

ATKINS

Telephone: +1.770.933.0280

Roberts Drive, Sidepath Scoping Study Project # S2123-1 City of Sandy Springs Public Works Department

I. Coordination/Meetings

- Project Kick-off meeting held on- 3/21/23
- 2. Atkins to provide project update to CoSS PM on -4/7/23 (mid-month)
 3. Atkins and CoSS to hold monthly coordination meetings- next conference
- call/coordination meeting to be held on 4/21/23

- 1. CoSS has provided the following reports

 - a. North End Blvd Study Final Report
 b. North End Blvd Scoping Study
 c. AECOM- Roberts Drive Under SR 400 Streetscape Section

- d. AECON-ROBER DIVE Office as A 400 Streetscape Section
 d. Preliminary Alternatives Assessment Study for NPS
 2. Review of current reports and study findings provided by CoSS
 3. Atkins requested GIS data base information—Pending GIS deliverables from the city.
 4. Atkins is working on preparing base information for side path study
 5. Initial site review of Roberts Drive corridor and potential side path alignment, date: 4-3-23
- Atkins internal coordination with Environmental Group. to begin "Desktop" review of the Corridor.

III. Deliverables

- Revised Schedule: see attached
 Initial Conceptual Plan side-path alignment alternative-TBD, Pending -GIS deliverables

ATKINS

Atkins North America, Inc. 1600 RiverEdge Parkway, NW, Suite 700 Atlanta, Georgia 30328

Telephone: +1.770.933.0280

Roberts Drive, Side Path Scoping Study Project # S2123-1 City of Sandy Springs Public Works Department Project Status Report #4 Meeting Minutes Date 6-22-23

Attendees

134

- Mr. Allen Johnson, TSPLOST Manager
- Mr. David Sustaita, CIP Project Manager
- Mr. Wesley Waters, CIP Unit Manager
- Ms. Ko Seo, CIP Project Manager
- Mr. Richard Rohrer, Atkins, Landscape Architect
- Mr. John Boudreau, Atkins, Project Manager

Atkins review the status of current project activities

- 1. Coss suggested Atkins reach out to GIS Dept to obtain more accurate GIS tonographic information
- 2. Atkins reviewed side path alignment starting on the north end of the project. A summary of the current alignment includes:
 - a. Atkins reviewed the side path alignment on the relocated and improved Roberts Drive. b. Atkins reviewed the potential utility relocations, including overhead pole
 - and fire hydrant relocation's locations. c. Atkins reviewed the potential stream crossing locations, including where
 - potential boardwalk and culvert improvements are proposed. d. Atkins reviewed the cost comparison between the concrete boardwalk and
 - the pedestrian bridge
 - Atkins reviewed wall locations
 - f. Atkins reviewed shifting of Roberts Drive to avoid the National Park Services historical civil war trench. Atkins indicated that the roadway centerline may have to be shifted towards the west, approximately 4'+-.
 - g. Atkins reviewed Roberts Drive roadway diet locations proposed at the larger neighborhood entrance to minimize impacts to neighborhood entrance walls
 - h. Atkins indicated that they would begin reviewing and identifying the proposed retaining wall lengths and heights to determine future potential cost implications.
 - i. Atkins reviewed that there is a potential temporary/permanent construction easement required from the NPS to construct the culvert and side path.
 - j. Atkins mid-block pedestrian crossing locations along Roberts Drive.

ATKINS

Atkins North America, Inc. 1600 RiverEdge Parkway, NW, Suite 700 Atlanta, Georgia 30328

Telephone: +1.770.933.0280

Roberts Drive, Side Path Scoping Study Project # S2123-1 City of Sandy Spring Public Works Departm Project Status Report #2 Date 5-8-23

- Mr. Allen Johnson, TSPLOST Manager
- Mr. David Sustaita, CIP Project Manager
- · Mr. Wesley Waters, CIP Unit Manage
- Ms. Ko Seo, CIP Project Manager
- Mr. Richard Rohrer, Atkins, Landscape Architect . Mr. John Boudreau, Atkins, Project Manager

Atkins review the status of current project activities:

- . "Desk Top Review" -underway anticipated competition of Desk Top review by 5/12/23
- 2. Atkins reviewed preferred and current side path alignment. A summary of the
- a) The side-path is proposed to remain on the outside of Roberts Drive from the southern to the northern end of the project limits. This is to minimize mid-block crossings and future safety concerns.
- b) Atkins expressed that locating the side path on the outside of Roberts Drive appears to be less overall impact to existing neighborhoods and its future location appears to make for a more pleasant pedestrian and bicycle experience.
- c) To minimize impacts to the neighborhoods and National Park property, Atkins proposing to minimize Roberts Drive roadway and, travel lane width. The proposed lane width is to be a min of 11'-0". The City agreed with Roberts Drive lane width reduction and roadway diet in difficult areas. The goal would be to hold the inside curb line alignment as much as possible and reduce the outside roadway to minimize
- the impact to adjacent properties.
 d) Currently the Roberts Drive width varies and is +/- 26' from face of curb to face of
- e) The typical side path section is to include a 5' grass strip from the back of curb to the face of the 12' wide side path
- f) The 12' wide side path is proposed to be concre
- g) The minimum side path section would include a 2' grass strip and 8' wide side path b) When the width of the grass strip has been reduced to 2' offset from the back of curb, the 2' offset is to include either a guardrail or shrub planting.
- Atkins reviewed the potential shifting of Roberts Drive towards the east to avoid the National Park Services historical civil war trenches. Atkins indicated that the roadway would have to be shifted towards the east, approximately 4'-5'. The shift would

ATKINS

Atkins North America, Inc. 1600 RiverEdge Parkway, NW, Suite 700 Atlanta, Georgia 30328

Telephone: +1.770.933.0280

Project # S2123-1 City of Sandy Springs Public Works Department Project Status Report #4 Meeting Minutes

Roberts Drive, Side Path Scoping Study

- Mr. Allen Johnson, TSPLOST Manager
- Mr. David Sustaita, CIP Project Manager Mr. Wesley Waters, CIP Unit Manager
- . Ms. Ko Seo, CIP Project Manager
- · Mr. Richard Rohrer, Atkins, Landscape Architect
- Mr. John Boudreau, Atkins, Project Manager

Atkins review the status of current project activities:

- 1. CoSS suggested Atkins reach out to GIS Dept to obtain more accurate GIS
- topographic information.
- 2. Atkins reviewed the further development of the side path alignment. A summary of the current alignment includes:
 - a. Atkins reviewed road diet at historic property location.
 - b. Atkins reviewed the potential utility relocations, including overhead pole and fire hydrant relocation's locations.
 - c. Atkins reviewed the potential stream crossing locations, including where potential boardwalk and culvert improvements are proposed.
 - d. Atkins reviewed the cost comparison between the concrete boardwalk and the pedestrian bridge
 - e. Atkins reviewed proposed wall locations
 - f. Atkins reviewed shifting of Roberts Drive to avoid the National Park Services historical civil war trench. Atkins indicated that the roadway centerline may have to be shifted towards the west, approximately 4'+-.
 - g. Atkins reviewed Roberts Drive roadway diet locations proposed at the larger neighborhood entrance to minimize impacts to neighborhood entrance walls and signage features.
 - h. Atkins indicated that they would begin reviewing and identifying the proposed retaining wall lengths and heights to determine future potentia cost implications.
- i. Atkins reviewed that there is a potential temporary/permanent construction easement required from the NPS to construct the culvert and side path.
- Atkins reviewed the mid-block pedestrian crossing locations along Roberts

ROBERTS DRIVE SIDE PATH SCOPING REPORT

COST ESTIMATE

ROBERTS DRIVE - Phase 1 Dunwoody Place past Island Ford Parkway (Entrance)Station 00+29 to Station 53+10 (5,281 LF = 1 mile) Date: 1/02/2024

PHASE 1, SHEET 1 OF 2

Dec	TEEM Demolition and Grading Items raffic Control rading Complete Learing and Grubbing Lemove Concrete Curb & gutter Lemove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing onstruction exit construct and Remove temporary sediment barrier or baled straw check dam	road diet sta. 0+00 to 8+12 + intersections road diet sta. 0+00 to 8+12 (760 LF)x 11 ft wide = 8360 sf (930 SY)	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	LS AC LF	\$75,000.00 \$450,000.00 \$1,500.00 \$6.50	\$75,000 \$450,000 \$2,250
De De De De De De De De	temolition and Grading Items rardic Control rardinc Complete learing and Grubbing temove Concrete Curb & gutter temove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SLAB AJOUST MANUGET OF GRADE rosion and Sediment Control emporary grassing onstruction exit construct and Remove temporary sediment barrier or baled straw check dam	road diet sta. 0+00 to 8+12 + intersections	1 1 1.5 1000 180 13	LS LS AC LF	\$75,000.00 \$450,000.00 \$1,500.00	\$75,000 \$450,000 \$2,250
S0-1000	raffic Control frieding Complete learing and Grubbing lemove Concrete Curb & gutter lemove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing onstruction exit construct and Remove temporary sediment barrier or baled straw check dam	road diet sta. 0+00 to 8+12 + intersections	1 1.5 1000 180 13 1000	LS AC LF	\$450,000.00 \$1,500.00	\$450,000 \$2,250
01-1500 Cle 10-0355 Rei 10-2815 Rei 10-2815 Rei NDEF 09-1000 11-8050 Er 63-0232 Ter 63-0329 Cor 63-0529 Cor 06-0010 Ma 65-0101 Ma 65-0105 Inl 65-0105 Inl 65-0105 Inl 65-0105 Inl 63-0329 Ter 71-0030 Ter 71-0030 Ter 73-2182 Str	temove Concrete Curb & gutter temove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing construction exit construct and Remove temporary sediment barrier or baled straw check dam		1.5 1000 180 13 1000	AC LF	\$1,500.00	\$2,250
10-0355 Rei 10-2815 Rei 10-281	temove Concrete Curb & gutter temove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE Tosion and Sediment Control emporary grassing onstruction exit onstruction exit onstruct and Remove temporary sediment barrier or balled straw check dam		1000 180 13 1000	LF		
10-2815 Res NDEF 09-1000 11-8050 Er 63-0232 Tes 63-0300 Co 63-0329 Co 63-0550 Ma 65-0101 Ma 65-0105 Inle 17-0300 Filt 03-2182 Str 43-8200 Bat	Remove Concrete sidewalk Remove ex. Ramp REMOVE ROADWAY SIAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing construction exit onstruct and Remove temporary sediment barrier or baled straw check dam		180 13 1000		\$6.50	\$6,500
NDEF	Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing construction exit onstruct and Remove temporary sediment barrier or baled straw check dam	road diet sta. 0+00 to 8+12 (760 LF)x 11 ft wide = 8360 sf (930 SY)	13 1000	SY		
NDEF	Remove ex. Ramp REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing construction exit onstruct and Remove temporary sediment barrier or baled straw check dam	road diet sta. 0+00 to 8+12 (760 LF)x 11 ft wide = 8360 sf (930 SY)	13 1000	SY		
09-1000	REMOVE ROADWAY SLAB ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing ionstruction exit onstruction exit	road diet sta. 0+00 to 8+12 (760 LF)x 11 ft wide = 8360 sf (930 SY)	1000	634	\$6.50	\$1,170
11-8050 Erc 63-0232 Ter 63-0300 Coi 63-0529 Coi 63-0550 Ma 65-0010 Ma 65-0105 Inle 55-1000 Filt 35-1000 Filt 343-8200 Bai	ADJUST MANHOLE TO GRADE rosion and Sediment Control emporary grassing construction exit construction exit	road diet sta. 0+00 to 8+12 (760 LF)x 11 ft wide = 8360 sf (930 SY)			\$6.50	\$84
Erc 63-0232 Ter 63-0300 Cot 63-0529 Cot 63-05529 Cot 65-0010 Ma 65-0101 Ma 65-0101 Inle 71-0030 Ter 55-1000 Filli 03-2182 Str 43-8200 Bas	rosion and Sediment Control emporary grassing onstruction exit construction exit construct and Remove temporary sediment barrier or baled straw check dam				\$100.00	\$100,000
63-0232 Ter 63-0300 Cor 63-0529 Cor 63-0550 Ma 65-0101 Ma 65-0105 Inla 77-0030 Ter 55-1000 Filtb 03-2182 Str 43-8200 Bai	emporary grassing ionstruction exit onstruct and Remove temporary sediment barrier or baled straw check dam		, 5	EA	\$2,000.00 Subtotal	\$10,000 \$645,004
63-0300 Coi 63-0529 Cor 63-0550 Ma 65-0101 Ma 65-0105 Inle 71-0030 Ter 55-1000 Filt 03-2182 Str 43-8200 Bai	onstruction exit onstruct and Remove temporary sediment barrier or baled straw check dam				Subtotui	\$0.13 ,00.11
63-0529 Coi 63-0550 Ma 65-0010 Ma 65-0101 Ma 65-0105 Inle 71-0030 Ter 55-1000 Filt 03-2182 Stn 43-8200 Bai	construct and Remove temporary sediment barrier or baled straw check dam	length of project x 10 ft width	1.2		\$600.00	\$720
63-0550 Ma 65-0010 Ma 65-0101 Ma 65-0105 Inlu 71-0030 Ter 55-1000 Fillt 03-2182 Stn 43-8200 Bai			1000		\$2,000.00 \$7.00	\$4,000 \$7,000
65-0101 Ma 65-0105 Inle 71-0030 Ter 55-1000 Filt 03-2182 Stn 43-8200 Bai	Aaintain Inlet Sediment Trap	ex. Storm inlets	9		\$225.00	\$2,025
65-0105 Inle 71-0030 Ter 55-1000 Filt 03-2182 Stn 43-8200 Bar	Aaintenance of Temp Silt Fence -		5000		\$3.00	\$15,000
71-0030 Ter 55-1000 Filt 03-2182 Stn 43-8200 Bar	Maintenance of Construction Entrance - nlet Sediment Trap maintainance		2 9		\$750.00 \$150.00	\$1,500 \$1,350
55-1000 Filt 03-2182 Stn 43-8200 Bai	emporary Silt Fence, Type C	length of project = 5, 281 LF	5000		\$5.00	\$25,000
43-8200 Bai	ilter Fabric for Embankment Stabilization	EST.	1000	SY	\$6.00	\$6,000
	tn Dumped Rip Rap, TP, 3, 24"		100		\$70.00	\$7,000
	arrier Fence (Orange), 4ft 'ermanent grassing	length of project x 10 ft width	5000 1.2		\$3.00 \$1,250.00	\$15,000 \$1,500
					Subtotal	\$86,095.
Sid	ide Path and Walls					
41-0104 Cor	ionc. Sidewalk 4 IN		105	SY	\$65.00	\$6,825.
NDEF		Dunwoody Place entry plaza	950		\$15.00	\$14,250.
NUCF	Brick pavers	Dunwoody Fidte entry plaza	950	3F	\$15.00	\$14,250.
		sht 1=(833 SY) + sht 2=(992 SY) + sht 3=(1276 SY) + sht 4=(70 SY) + sht 5=(146				
		SY) + sht 6 = (62 SY) = 3379 sy for project				
	conc. Sidewalk 6 IN		3500		\$95.00	\$332,500.
10-5040 GR	SR AGGR BASE CRS, 4 INCH, INCL MATL	sidewalks + under pavers (plaza)	3500	SY	\$35.00	\$122,500.
Wa	Valls				Subtotal	\$476,075.
	Vall #1 Poured in Place Wall w/ picket fencing (separate line item)	sta. 0+40 to 3+10 (270 lf, avg. ht.6 ft , 12" width)	70	CY	\$1,500.00	\$105,000.
07-3001 Wa	Vall #1 Stone Cap	12" x 24" cap	300		\$35.00	\$10,500.
	Vall #1 Stone Veneer	granite rubble, ashlar pattern	1620		\$35.00	\$56,700.
	Vall #2 Turnup Concrete Slab with Reinforcement Vall #2 Stone Cap	sta. 6+36 to 8+12(176 lf, 2.5'avg ht x 8" width) 12" x 24" cap	17 200		\$1,650.00 \$35.00	\$28,050. \$7,000.
	Vall #2 Stone Veneer	granite rubble, ashlar pattern	440		\$35.00	\$15,400.
00-3110 Wa	Vall #3 Turn-down slab w/cable rail (separate line item)	sta.37+24 to 38+77(153 LF x 4-6ft avg.ht.x 8" width)	23	CY	\$1,650.00	\$37,950.
			105		\$1,650.00	\$173,250.
70-3110 W	Vall #4 Turn-down slab w/cable rail (separate line item)	sta.40+22 to 47+33(711 lf, 4-5ft avg. ht. x 8" width)	103	Ci	Subtotal	\$433,850.
41-7011 Cui	Curb Cut Wheelchair Ramp, Type A		11	EA	\$2,800.00	\$30,800.
	urb Cut Wheelchair Ramp, Type B		0		\$3,700.00	\$0.
	urb Cut Wheelchair Ramp, Type C urb Cut Wheelchair Ramp, Type D		7		\$2,400.00 \$2,000.00	\$0. \$14,000.
11-7014 Cui	and cut wheelchan ramp, type o		· · · · · · · · · · · · · · · · · · ·		32,000.00	\$14,000 .
99-5200 DE	ETECTABLE WARNING SURFACE		144	SF	\$35.00	\$5,040.
					Subtotal	\$49,840.
Sig 36-6025	ignage on Side path Bicycles Only	ESTIMATED	4	EA	\$1,000.00	\$4,000.
36-6026	Wayfinding Signage	Committee	0		\$1,000.00	\$0.
36-6027	Mile Markers		0		\$1,000.00	\$0.
52-9002	Yellow Centerline		0	LF	\$3.00	\$0.
Ro	toadway				Subtotal	\$4,000.0
	Concrete Driveway Apron (8" Thick)	Island Ferry Road	30	SY	\$125.00	\$3,750.
	Concrete Curb and Gutter 8" x 30" Tp 2	includes sta. 1+00 to 7+00 and radius at intersections	1000		\$30.00	\$30,000.
	tecycled Asph Conc 9.5 MM Superpave, Type II, GP 2 Only, Incl Bituminous Matl & H Lime (2 inch) tecycled Asph Conc 12.5 MM Superpave, GP 2 Only, Incl Matl & H Lime (1.5 inch)		110 83		\$205.00 \$200.00	\$22,550.0 \$16,600.0
	//ILL ASPH CONC PVMT, VARIABLE DEPTH		0		\$20.00	\$0.
Sig	igning and Marking					
NDEF	Pedestrian crossing signal pole	relocate from plaza at Dunwoody Pl.	1	EA	\$500.00	\$500.
NDEF	Stop Sign		3	EA	\$1,200.00	\$3,600.
NDEF	Multiple traffic signs	roadway	5	EA	\$1,200.00	\$6,000.
52-5701	Stop Bar	125 LF drive/road stop bars	125	LF	\$6.00	\$750.
52-9002	Yellow Centerline	road diet sta. 0+00 to 8+12 + intersections	922	LF	\$3.00	\$2,766.
1ISC	Rectangular Rapid Flashing Beacon	Pride Place (sta.12+50) & Island Ford Parkway Entrance (sta.34+05)and sta. 52+44	922		\$25,000.00	\$2,766.
41-0748	Concrete Median 6"	sta 52+44 (Sheet 6)	1		\$20,000.00	\$20,000
NDEF	Brick pavers	mid-block crossing median (on conc. Base; qty added to conc. Item 40 sy)	360	SF	\$15.00	\$5,400
	Itilities			أنيورا		
68-4300 Pro	roposed Storm Manhole		1		\$2,500.00	\$2,500
	Orop Inlet GP1 GDOT 1019A		1 15		\$3,500.00	\$3,500
	torm Pipe 18" RCP torm Pipe 24" RCP		15		\$75.00 \$100.00	\$1,125 \$0
50-1181 Sto	Oncrete End Section		0		\$5,000.00	\$0.
50-1181 Sto 50-1240 Sto	TN PLAIN RIP RAP 24					
50-1181 Sto 50-1240 Sto INDEF Cor 03-1024 STR	TN PLAIN RIP RAP 24 irehydrant Relocation		0		\$85.00 \$3,500.00	\$0. \$0.

COST ESTIMATE

Dunwoody Place past Island Ford Parkway (Entrance)
Station 00+29 to Station 53+10 (5,281 LF = 1 mile)
Date:1/02/2024

PHASE 1, SHEET 2 OF 2

Ste Amenities	ITEM CODE	ITEM		NUMBER	UNIT	COST/UNIT	TOTAL
Section		Site Amenities		Nomber	O.I.I.	costyoun	TOTAL
March 1998							\$22,500.0
Table Tabl	641-2200	Picket Fencing -top of walls > 6' ht.	48" Picket Fence	30	O LF	\$125.00	\$37,500.0
Part		Cable Handrailing -top of turn-down slabs	42" Cable Handrailing	87			\$174,000.0
Transfer Corner Springe Interfer Corner							\$8,000.0
Section Sect	UNDEF						\$2,500.0
Part Topped Aft Part			at midblock crossing locations		6 EA	\$500.00	\$3,000.0
20 20 20 20 20 20 20 20	708-1000		4" for sod areas + plants	9	O CY	\$50.00	\$4,500.0
Part			, and the same of	-		7	+ 1,0000
Part	702-9025	Landscape Mulch	4" depth	67	7 SY	\$20.00	\$13,540.0
Paints			·				
Paints							
Paints							
Part							
MICHE Street Press	700-9300	Sod	Bermuda	12	3 SY	\$15.00	\$1,845.0
Section Sect							
							\$9,000.0
Name Outside Construction Contract	UNDEF	Shrubs - 3' o.c.	3 gal., variety TBD	193	3 EA		\$77,320.0
Comparison Contract Contrac						Subtotal	\$353,705.00
Committee Contract						Project Subtetal	\$2,242,610.50
Name Outside Construction Contract Name Outside Construction Contract				O&P/Proj Mamt/N	Anhilization/Bor		\$560,652.63
Design & Engineering Service (123) Sacron Total S				Odi / i Toj ivigilit/ii	lobilization, boi		\$448,522.1
					Design & Eng		\$269,113.20
							\$3,520,898.49
Mail: Mail							
Mail: Mail							***************************************
44.1015 Relocate Unity Pole		Items Outside Construction Contract					
431-0105 Relocate Light Pole 431-0105 Relocate Light Pole (131 decornitive) 1 pole approx. 75 ft. oc. 54 EA 512,000.00							
Mat-0105 And Pedestrian Light Pole (13' decorative) 1 pole approx. 75 ft. o. C. (15 8+12 to 22+17) 17							\$200,000.00
Add Prediction Light Polic (13" decorative) GOOT segment 1 10 10 20 20 20 20 20							\$0.00
MAIL-0105 And Predestrian Light Pole (18f decorative gateway) 1 g sta. 0-75 Durwoody Pi. + 2 g Island Ford Parkway 4 EA S15,000.00 882-950 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-959 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-959 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-959 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 50 lb bore per under roadway 700 LF \$20.00 882-199 Directional Bore 700 LF \$20.00 882-199 Directiona	441-0105	Add Pedestrian Light Pole (13' decorative)	1 pole approx. 75 ft. o.c.	5	4 EA	\$12,000.00	\$648,000.00
632-850 Electrical Service Point		Add Pedestrian Light Pole (13' decorative) GDOT segment	1 pole approx. 75 ft. o.c. (sta 8+12 to 22+17)	1	7 EA	\$12,000.00	\$204,000.00
S82-950 Directional Bore S02-050 Directional Bore S02-050 Directional Bore S02-050 Directional Bore S02-050 EA S15.00			1 @ sta. 0+75 Dunwoody Pl. + 2 @ Island Ford Parkway				\$45,000.00
\$22-252 Conduit NOMMETL, TP. 2, 2 Man diffect. Wire \$500 EA \$15.00 \$28-23.30 \$26.00 TPTM AMOR NOT 130 \$28.25.50 \$15.00 \$28.25.50 \$15.00 \$28.25.50 \$15.00 \$28.25.50 \$15.00 \$28.25.50 \$15.00 \$28.25.50 \$15.00 \$28.25.50 \$29.25.50							\$40,000.00
82-21-33			50 If bore per under roadway				\$14,000.00
Substant							\$75,000.00 \$50,000.00
Subtotal			to be determined				\$0.00
Right-Of-Way and Easement Cost	002 0323	neroedic Erectrical Dox	to be determined		, Ex		\$1,276,000.00
Right-Of-Way Cost							
Industrial PA# 06 0364 LL0404 (corner plaza at Dunnwoody PI @ sta. 0+29) 387 \$7 \$35.00						<u>'</u>	
Single Family Residential SF \$20.00							
Multi-Family Residential -Apartments SF \$50.00			PA# 06 0364 LL0404 (corner plaza at Dunnwoody PI @ sta. 0+29)	38			\$13,545.00
Multi-Family Residential - Condominiums SF \$30.00							\$0.00
Multi-Family Residential - Townhomes SF \$20.00							\$0.00 \$0.00
Permanent Easement (50% of ROW) Industrial							\$0.00
Industrial SF \$17.50 Single Family Residential - Apartments SF \$10.00 Multi-Family Residential - Condominiums SF \$25.00 Multi-Family Residential - Townhomes SF \$15.00 Multi-Family Residential - Townhomes SF \$15.00 PAB 060365-LL0270 (state property storm culvert @ sta. 51+00) 771 SF \$5.00 Temp Construction Easement (20% of ROW) Industrial SF \$7.00 Single Family Residential - Apartments SF \$4.00 Multi-Family Residential - Apartments SF \$4.00 Multi-Family Residential - Apartments PAB 060364-LL0511 (summerbrook @ sta. 23+63 to 31+20) 3785 SF \$4.00 Multi-Family Residential - Townhomes SF \$4.00 Multi-Family Residential - Townhomes SF \$4.00 Public Property PAB 060365-LL0270 (state property @ sta. 31+20 to 32+72) 760 SF \$5.00 PAB 060365-LL0270 (state property @ sta. 31+20 to 32+72) 760 SF \$5.00 PAB 060365-LL0270 (state property @ sta. 36+59) 1545 SF \$5.00 Driveway Easement Drivew	***************************************						
Single Family Residential - Apartments		Permanent Easement (50% of ROW)					
Multi-Family Residential - Apartments							\$0.00
Multi-Family Residential - Condominiums							\$0.00
Multi-Family Residential - Townhomes PA# 060365-LL0270 (state property storm culvert @ sta. 51+00) 771 5F \$0.00							\$0.00
Public Property PA# 060365-LL0270 (state property storm culvert @ sta. 51+00) 771 55 50.00 Temp Construction Easement (20% of ROW)							\$0.00
Temp Construction Easement (20% of ROW) Industrial			PA# 060365-LL0270 (state property storm culvert @ cts 51±00)	77			\$0.00 \$0.00
Industrial			1 Am 000303-120270 (state property storin curvert @ stat 51+00)		_ or	ŞU.UU	30.00
Single Family Residential SF \$4.00					SF	\$7.00	\$0.00
Multi-Family Residential -Apartments					SF	\$4.00	\$0.00
Multi-Family Residential - Townhomes		Multi-Family Residential -Apartments	PA# 060364-LL0511 (Summerbrook @ sta. 23+63 to 31+20)	378		\$10.00	\$37,850.00
Public Property PA# 060365-LL0270 (state property @ sta. 31+20 to 32+72) 760 SF \$0.00							\$0.00
PAB 060365-LL0270 (state property @ sta. 31+20 to 32+72) 760 SF \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$0.00 \$ \$ \$0.00 \$ \$ \$0.00 \$ \$ \$ \$ \$ \$ \$ \$ \$					SF	\$4.00	\$0.00
(sta.33+50 to 36+59) 1545 \$F \$0.00		Public Property	DA# 060265_110270 (state property @ stat 24+20 to 22+72)		n cr	ćn 00	\$0.00
PAB 060365-LI02762 (city property @ sta. 36+59 to 46+82) 5115 \$5 \$0.00	***************************************						\$0.00
PA# 060365-LL0270 (state property @ sta. 46+82 to 53+10) 3140 5F \$0.00 Driveway Easement							\$0.0
Driveway Easement no charges Public Property (City) Island Ferry Rd 1002 SF \$0.00 Subtotal							\$0.0
Subtotal		Driveway Easement					
Subtotal	no charges	Public Property (City)	Island Ferry Rd	100	2 SF		\$0.0
Note: The Conceptual Cost Estimate is based on Conceptual Plan and is approximate cost only.						Subtotal	\$51,395.00
Note: The Conceptual Cost Estimate is based on Conceptual Plan and is approximate cost only.							
		Note: The Conceptual Cost Estimate is based on Conceptual Plan and is approximate cost only.					

136 ROBERTS DRIVE SIDE PATH SCOPING REPORT

COST ESTIMATE

ROBERTS DRIVE - Phase 2A
Highway 9 / Roswell Road to end of Phase 1
Station 138+10 = (6,585 LF = 1.2 r PHASE 2 ALTERNATIVE, SHEET 1 OF 2

ITEM CODE	ITEM		NUMBER	UNIT	COST/UNIT	TOTAL
	Demolition and Grading Items	Notes		·		
50-1000	Traffic Control		1	LS	\$150,000.00	\$150,000.0
10-0100	Grading Complete		1	LS	\$1,250,000.00	\$1,250,000.0
01-1500	Clearing and Grubbing		1.5	AC	\$1,500.00	\$2,250.0
10-0300	Remove wood fence in R/W	sta 105+00 to 103+28 (exclude drive)	107	LF	\$25.00	\$2,675.0
JNDEF	Remove Ex. Tree	sta 86+55 and sta81+25 (Lexington Dr. entry)	2	EA	\$1,000.00	\$2,000.0
10-2815	Remove existing Sidewalk	Rivermere Way and Roberts Landing Cove 2 at The Lodge at Rivercrest Dr., 4 block columns stone veneer, lighted(sta 84+50 and 85+00) + 2 at Roberts Landing Cove, 4 brick columns aluminum picket fending, brick knee wall(sta 83+00-84+00) + 2 at Lexington Dr., 5 brick columns, wood fence,	4155	SY	\$6.50	\$27,007.5
510-9097	Remove sign walls and fence in R/W	aluminum sign (sta 79+50 - 81+00)	6	EA	\$10,000.00	\$60,000.0
510-0355 511-8050	Remove entrance curb & gutter ADJUST MANHOLE TO GRADE	Rivercrest Drive, Roberts Landing Cove & Lexington Dr.	300 4	LF EA	\$6.50 \$2,000.00	\$1,950.0 \$8,000.0
011-8030	ADJOST MANTOLE TO GRADE		4	LA	Subtotal	\$1,503,882.5
	Erosion and Sediment Control					
63-0232	Temporary grassing	10 sf x length	1.5	AC	\$600.00	\$900.0
.63-0300	Construction exit		2	EA	\$2,000.00	\$4,000.0
.63-0529	Construct and Remove temporary sediment barrier or baled straw check dam		500	LF	\$7.00	\$3,500.0
.63-0550	Maintain Inlet Sediment Trap		7	EA	\$225.00	\$1,575.0
65-0010	Maintenance of Temp Silt Fence -		6500	LF	\$3.00	\$19,500.0
65-0101	Maintenance of Construction Exit		7	EA	\$750.00	\$1,500.0
65-0101 71-0030	Inlet Sediment Trap Maintenance Temporary Silt Fence, Type C		6500	EA LF	\$150.00 \$5.00	\$1,050.0 \$32,500.0
55-1000	Filter Frabic for Embankment Stablization	EST.	1000	SY	\$6.00	\$6,000.0
03-2182	Stn Dumped Rip Rap, TP, 3, 24"	LUI.	100	SY	\$70.00	\$7,000.0
43-8200	Barrier Fence (Orange), 4ft		6500	LF	\$3.00	\$19,500.
00-6910	Permanent grassing		1.5	AC	\$1,250.00	\$1,875.0
		<u> </u>	1.5	,,,,	Subtotal	\$98,900.0
	Side Path and Concrete Walk				1	
41-0104	Conc. Sidewalk 4 IN	4" under pavers only	67	SY	\$65.00	\$4,355.0
	Brick pavers	Roswell Rd. entry plaza	602	SF	\$15.00	\$9,030.0
		,,,,,,			1	
141-0106	Conc. Sidewalk 6 IN		6646	SY	\$95.00	\$631,370.0
10-5040	GR AGGR BASE CRS, 4 INCH, INCL MATL		6646	SY	\$35.00	\$232,610.0
					Subtotal	\$877,365.0
500-3120	Walls Wall #1 Turnup Concrete Slab with Reinforcement	sta, 117+98' to 116+67 (131 lf, avg.2-3' hgt. x 8" wide)	10	CY	\$1,550.00	\$15,500.0
	Wall #1 Stone Cap	12" x 24" cap	140	LF	\$35.00	\$4,900.0
07-3001		granite rubble, ashlar pattern	393	SF	\$35.00	\$13,755.0
507-3001 508-3000	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement	granite rubble, ashlar pattern sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide)	393 4	SF CY	\$35.00 \$1,550.00	
607-3001 608-3000 600-3120	Wall #1 Stone Veneer					\$6,200.0
507-3001 508-3000 500-3120 507-3001 508-3000	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide)	4 85 162	CY LF SF	\$1,550.00	\$13,755.0 \$6,200.0 \$2,975.0 \$5,670.0
607-3001 608-3000 600-3120 607-3001 608-3000	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap	4 85	CY LF	\$1,550.00 \$35.00	\$6,200.0 \$2,975.0 \$5,670.0
507-3001 508-3000 500-3120 507-3001 508-3000 500-3120	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162	CY LF SF	\$1,550.00 \$35.00 \$35.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0
507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide)	85 162 5	CY LF SF CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0 \$3,325.0
507-3001 508-3000 500-3120 507-3001 508-3000 507-3001 508-3000	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap	4 85 162 5 95	CY LF SF CY LF	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0 \$3,325.0
507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001 508-3000 500-3120	Wall #3 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95	CY LF SF CY LF	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0 \$3,325.0 \$6,440.0 \$102,200.0
507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 184 73	CY LF SF CY LF SF CY LF SF CY LF SF	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0 \$3,325.0 \$6,440.0 \$102,200.0 \$4,200.0
507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 508-3000 500-3120 508-3000 500-3120	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide)	4 85 162 5 95 184 73 120 680	CY LF SF CY LF SF CY LF SF CY CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,550.00	\$6,200.0 \$2,975.0 \$5,670.0 \$7,750.0 \$3,325.0 \$6,440.0 \$102,200.0 \$4,200.0 \$23,800.0 \$15,500.0
507-3001 508-3000 509-3120 507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 507-3001 508-3000 500-3120 500-3120 507-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Veneer Wall #4 Stone Veneer Wall #4 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap	4 85 162 5 95 184 73 120 680 10	CY LF SF CY LF SF CY LF SF CY LF	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850.
607-3001 608-3000 600-3120 607-3001 608-3000 600-3120 607-3001 608-3000 600-3120 608-3000 600-3120 600-3120 600-3120 600-3120 600-3120 600-3120 600-3120 600-3120 600-3120	Wall #3 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Veneer Wall #5 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 184 73 120 680 10 110 318	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130.
107-3001 108-3000 108-3000 100-3120 107-3001 108-3000 107-3001 108-3000 100-3120 107-3001 108-3000 100-3120 107-3001 108-3000 100-3120 107-3001 108-3000 100-3120 107-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 120 680 10 110 3188 54	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$1,550.00 \$35.00 \$1,450.00	\$6,200.0 \$2,975.1 \$5,670.0 \$7,750.0 \$3,325.0 \$102,200.0 \$4,200.1 \$23,800.0 \$3,850.0 \$3,850.0 \$3,850.0 \$3,850.0 \$3,750.0 \$4,200.1
107-3001 108-3000 108-3000 109-3120 107-3001 108-3000 107-3001 108-3000 107-3001 108-3000 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Cap Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap	4 85 162 5 95 184 73 120 680 100 1110 3188 54 55	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$23,800. \$15,500. \$3,850. \$11,130. \$75,600.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-301 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #5 Stone Veneer Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 184 73 120 680 110 318 54 55 470	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$10,200. \$4,200. \$13,500. \$1,1130. \$75,600. \$1,925.
107-3001 108-3000 109-3100	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #6 Tone. Gravity Walls GDOT 9031L Wall #6 Stone Cap Wall #6 Tone Cap Wall #7 Cone. Gravity Walls GDOT 9031L w/picket railing	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 105+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide)	4 85 162 5 95 1844 73 120 680 10 110 318 54 55 470 410	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$11,130. \$11,250. \$1,925. \$16,450. \$574,000.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #5 Stone Veneer Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 184 73 120 680 110 318 54 55 470	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$75,600. \$1,925. \$16,450. \$574,000. \$19,320.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 00-3120 00-3120 00-3120 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-301 08-3000 00-3120 07-301 08-3000 00-3120 07-301 08-3000	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap	4 85 162 5 95 184 73 120 680 10 110 318 54 55 470 410 5552	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$1,5500. \$3,850. \$11,130. \$75,600. \$1,9320. \$19,320.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Veneer Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Veneer Wall #7 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 184 73 120 680 110 318 54 55 470 410 552 3600	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$1,400.00 \$35.00 \$1,550.00 \$35.00 \$1,400.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$75,600. \$1,925. \$16,450. \$19,320. \$126,000. \$77,000.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Veneer Wall #7 Stone Veneer Wall #7 Stone Veneer Wall #7 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 3 120 689 100 110 318 54 470 410 555 3600 55 119 480	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$1,400.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$3,880. \$15,500. \$3,850. \$15,500. \$3,850. \$15,500. \$1,925. \$16,450. \$574,000. \$19,320. \$126,000. \$77,000. \$4,165. \$16,600.
07-3001 08-3000 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #7 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #7 Stone Veneer Wall #7 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 100 110 318 54 470 410 552 3600 555 119 480 150	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$75,600. \$1,925. \$16,450. \$19,320. \$14,000. \$77,000. \$4,165. \$16,800.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 4 73 120 680 100 110 318 54 55 470 410 552 3600 55 119 480 150 265	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$1,400.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$15,500. \$11,130. \$75,600. \$19,320. \$14,650. \$77,000. \$4,165. \$16,800. \$210,000. \$9,275.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #9 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 10 110 318 54 55 470 410 552 3600 55 119 480 150 265	CY	\$1,550.00 \$35.00 \$35.00 \$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$3,880. \$15,500. \$3,881. \$75,600. \$1,925. \$16,450. \$19,320. \$12,6,000. \$77,000. \$4,165. \$16,800. \$9,275. \$4,650. \$9,275. \$4,650.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 08-3000 00-3120 08-3000 00-3120 07-3001	Wall #3 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #7 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #7 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #9 Stone Veneer Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Veneer Wall #9 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 10 110 318 54 55 470 410 552 3600 55 119 480 150 265	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$19,25. \$16,450. \$19,320. \$14,650. \$17,000. \$4,165. \$16,800. \$210,000. \$210,000. \$9,275. \$45,500.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #10 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+34 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 4 73 120 680 150 265 1300 60 100 162 5	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$1,400.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$23,800. \$15,500. \$1,1130. \$15,500. \$1,925. \$16,450. \$19,320. \$126,000. \$77,000. \$4,165. \$16,800. \$210,000. \$9,275. \$45,500. \$84,000.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #8 Stone Veneer Wall #7 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #7 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #8 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Veneer Wall #9 Stone Cap Wall #9 Stone Veneer Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Veneer Wall #10 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 120 680 10 110 3188 54 55 470 410 552 3600 555 119 480 150 265 1300 60 100	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$15,500. \$3,850. \$1,130. \$75,600. \$1,925. \$16,450. \$574,000. \$19,320. \$16,6800. \$210,000. \$210,000. \$24,165. \$3,500. \$3,275. \$4,5500. \$3,275.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #10 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 100 1110 3188 54 55 470 410 552 3600 555 119 480 150 265 1300 60 100 550 70	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$19,25. \$16,450. \$19,320. \$14,65. \$16,800. \$210,000. \$4,165. \$16,800. \$210,000. \$4,165. \$16,800. \$210,000. \$4,165. \$16,800. \$10,300. \$10,300. \$10,300. \$10,500. \$10
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #1 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Veneer Wall #5 Stone Veneer Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #6 Tone. Gravity Walls GDOT 9031L Wall #7 Stone Veneer Wall #7 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #7 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #9 Stone Cap Wall #10 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #10 Stone Cap Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #10 Stone Cap Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 120 680 10 110 3188 54 55 470 410 552 3600 555 119 480 150 265 1300 60 100	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$11,130. \$15,500. \$19,320. \$16,450. \$19,320. \$14,650. \$21,000. \$21,000. \$21,000. \$21,000. \$21,000. \$3,350. \$3,500. \$3,500. \$3,500.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Veneer Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #10 Stone Cap Wall #10 Stone Cap Wall #11 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide)	4 85 162 5 95 184 4 73 120 680 150 550 70 100 100 550 75 100 100 100 100 100 162 162 162 162 162 162 162 162 162 162	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$1,400.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$3,850. \$11,130. \$75,600. \$11,250. \$11,250. \$5,146,450. \$574,000. \$77,000. \$4,165. \$16,800. \$210,000. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #9 Stone Cap Wall #10 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #11 Stone Veneer Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #11 Stone Veneer Wall #11 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 120 680 10 110 3188 54 55 470 410 552 3600 550 119 480 150 60 100 550 70 100 600	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$19,25. \$16,450. \$19,320. \$14,650. \$21,000. \$21,000. \$21,000. \$3,500. \$19,250. \$19,850. \$19,250. \$19,800. \$21,000.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #8 Stone Veneer Wall #8 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Veneer Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #11 Conc. Gravity Walls GDOT 9031L w/picket railing Wall #11 Stone Veneer Wall #12 Turndown Concrete Slab with Reinforcement w/cable handrail Wall #13 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 100 110 3188 54 55 470 410 552 3600 55 119 480 150 265 1300 60 100 550 70 100 600 7 55 159	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$3,3850. \$11,130. \$75,600. \$19,320. \$19,320. \$19,320. \$16,450. \$210,000. \$77,000. \$4,165. \$16,800. \$210,000. \$3,500.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000	Wall #2 Stone Veneer Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #5 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Conc. Gravity Walls GDOT 9031L Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #11 Stone Cap Wall #13 Stone Cap	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf,5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx x 8" wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide)	4 85 162 5 95 1844 73 120 680 100 110 3188 54 55 470 410 552 3600 555 119 480 150 60 100 550 70 100 600 77 555	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$19,25. \$16,450. \$19,320. \$14,650. \$19,250. \$4,165. \$16,800. \$21,000. \$3,500. \$4,500. \$3,500.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #11 Stone Cap Wall #12 Turndown Concrete Slab with Reinforcement w/cable handrail Wall #13 Stone Cap Wall #14 Turnup Concrete Slab with Reinforcement Wall #14 Turnup Concrete Slab	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+78 to sta. 87+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (718 lf, 2-3' avg. htx 8" wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (718 lf, 2-3' avg. htx 8" wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide)	4 85 162 5 95 184 73 120 680 100 110 3188 54 55 470 410 555 3600 555 119 480 150 265 1300 60 100 550 70 100 600 7 555 150 450	CY	\$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,400.00 \$35	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$23,860. \$1,500. \$3,850. \$11,130. \$75,660. \$19,320. \$14,650. \$21,000. \$21,000. \$3,500. \$3,500. \$3,500. \$3,500. \$3,500. \$21,000. \$3,500
07-3001 08-3000 08-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Veneer Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #10 Stone Veneer Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #13 Stone Cap Wall #14 Stone Cap Wa	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84-62 to 83+75 (87 lf, 2-3' avg. ht x 8' wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+00 to 68+20 (200 lf, 3' avg. hgt.x 8' wide)	4 85 162 5 95 1844 73 120 680 100 110 318 54 55 470 410 552 3600 55 119 480 150 265 1300 60 100 550 70 100 600 7 55 159 150 450 150 150 150 150 150 150 150 150 150 1	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$3,850. \$11,130. \$75,600. \$19,325. \$16,450. \$574,000. \$19,325. \$16,450. \$210,000. \$77,000. \$4,165. \$16,800. \$210,000. \$3,500. \$19,230. \$3,500. \$19,230. \$3,500. \$3,5
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Trunpu Concrete Slab with Reinforcement Wall #2 Trunpu Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #5 Turnup Concrete Slab with Reinforcement Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #1 Stone Cap Wall #14 Stone Cap Wal	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx 8" wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+20 to 68+20 (200 lf, 3' avg. hgt.x 8" wide)	4 85 162 5 95 1844 73 120 680 100 1100 3188 54 55 470 410 552 3600 555 1199 480 100 550 100 600 77 55 150 450 450 155	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$23,800. \$15,500. \$3,850. \$11,130. \$19,25. \$16,450. \$19,320. \$21,000. \$21,000. \$3,500. \$1,925. \$4,165. \$16,880. \$3,500. \$1,925. \$4,500. \$1,925. \$4,500. \$1,000. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$3,500. \$21,000. \$3,500
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #3 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Cap Wall #3 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Conc. Gravity Walls GDOT 9031L W/picket railing Wall #6 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #10 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #12 Turndown Concrete Slab with Reinforcement Wall #12 Stone Cap Wall #13 Stone Cap Wall #13 Stone Cap Wall #13 Stone Cap Wall #14 Stone Cap Wall #15 Turnup Concrete Slab with Reinforcement Wall #14 Stone Cap Wall #15 Turnup Concrete Slab with Reinforcement Wall #15 Turnup Concrete Slab	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 87-10 to 64+24 (286 lf, 4' avg. hgt.x 8" wide)	4 85 162 5 95 184 73 120 680 10 110 3188 54 55 470 410 552 3600 555 119 480 150 60 100 600 7 155 150 600 77 555 159 150 600 77 555 159 200 6600 28	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$15,500. \$3,850. \$11,320. \$5,290. \$19,320. \$19,320. \$19,320. \$210,000. \$77,000. \$4,165. \$4,650. \$9,275. \$4,550. \$84,000. \$3,500. \$110,280. \$3,500. \$10,850. \$77,000. \$5,250. \$15,750. \$51,085. \$77,000. \$52,20. \$77,000.
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #11 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #13 Stone Cap Wall #14 Stone Cap Wall #15 Stone Cap Wall #15 Stone Cap Wall #15 Stone Cap Wall #16 Stone Cap Wall #17 Stone Cap Wall #18 Stone Cap W	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+62 to 83+75 (87 lf, 2-3' avg. ht x 8' wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+00 to 68+20 (200 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 60+10 to 64+24 (286 lf, 4' avg. hgt.x 8" wide)	4 85 162 5 95 1844 73 120 680 100 110 318 54 55 470 410 552 3600 55 119 480 150 60 100 550 70 100 600 7 55 150 450 450 150 268	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$3,850. \$1,130. \$15,500. \$1,932. \$11,130. \$75,600. \$19,320. \$14,655. \$16,450. \$210,000. \$9,275. \$4,655. \$3,500. \$19,250. \$19,320. \$10,800. \$10,800. \$10,800. \$10,800. \$3,500. \$3,500. \$3,500. \$19,250. \$19,250. \$19,250. \$19,250. \$19,250. \$15,750. \$21,000. \$10,850. \$77,000. \$5,250. \$15,750. \$23,250. \$15,750. \$23,250. \$15,750. \$23,250. \$15,750. \$23,250. \$14,400. \$43,400. \$43,400.
07-3001 08-3000 00-3120 07-3001	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Veneer Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Cap Wall #6 Stone Veneer Wall #6 Stone Veneer Wall #7 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Cap Wall #8 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Veneer Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #12 Turnup Concrete Slab with Reinforcement w/cable handrail Wall #13 Stone Cap Wall #13 Stone Cap Wall #13 Stone Cap Wall #13 Stone Cap Wall #14 Stone Cap Wall #15 Stone Cap Wall #15 Stone Veneer Wall #16 Stone Veneer Wall #17 Stone Cap Wall #18 Stone Cap Wall #18 Stone Cap Wall #18 Stone Veneer Wall #18 Stone Veneer Wall #18 Stone Cap Wall #15 Stone Veneer	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 5' avg. hgt.x8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (871 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (871 lf, 2-3' avg. htx 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+20 to 68+20 (200 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+20 to 68+20 (200 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 67+10 to 64+24 (286 lf, 4' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern	4 85 162 5 95 1844 73 120 680 10 110 3188 54 55 470 410 552 3600 555 119 480 150 60 100 550 70 100 600 7 555 155 159 200 600 28 286 1144	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$3,850. \$1,935. \$1,935. \$16,450. \$574,000. \$77,000. \$77,000. \$210,000. \$9,275. \$4,550. \$84,000. \$19,320. \$19,320. \$10,800. \$19,320. \$10,800. \$10,800. \$10,800. \$21,000. \$3,500. \$10,850. \$10,850. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$21,000. \$3,500. \$3
07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000 00-3120 07-3001 08-3000	Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Turnup Concrete Slab with Reinforcement Wall #2 Stone Cap Wall #2 Stone Cap Wall #3 Turnup Concrete Slab with Reinforcement Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #4 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #5 Stone Cap Wall #6 Stone Cap Wall #7 Stone Cap Wall #7 Stone Cap Wall #8 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #9 Stone Cap Wall #10 Stone Cap Wall #11 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Cap Wall #13 Stone Cap Wall #14 Stone Cap Wall #15 Stone Cap Wall #15 Stone Cap Wall #15 Stone Cap Wall #16 Stone Cap Wall #17 Stone Cap Wall #18 Stone Cap W	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 111+77 to sta. 110+85 (92 lf, 2' avg.hgt. x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg. 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 105+80 to sta. 104+74 (106 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+62 to 83+75 (87 lf, 2-3' avg. ht x 8' wide) sta. 77+70 to 76+21 (149 lf, 3' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 70+00 to 68+20 (200 lf, 3' avg. hgt.x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 60+10 to 64+24 (286 lf, 4' avg. hgt.x 8" wide)	4 85 162 5 95 1844 73 120 680 100 110 318 54 55 470 410 552 3600 55 119 480 150 60 100 550 70 100 600 7 55 150 450 450 150 268	CY	\$1,550.00 \$35.00	\$6,200. \$2,975. \$5,670. \$7,750. \$3,325. \$6,440. \$102,200. \$4,200. \$4,200. \$15,500. \$3,850. \$11,320. \$5,290. \$19,320. \$19,320. \$19,320. \$210,000. \$77,000. \$4,165. \$4,650. \$9,275. \$4,550. \$84,000. \$3,500. \$110,280. \$3,500. \$10,850. \$77,000. \$5,250. \$15,750. \$51,085. \$77,000. \$52,20. \$77,000.

07.4 COST ESTIMATE

PHASE 2 ALTERNATIVE, SHEET 2 OF 2

ITEM CODE							
	ITEM		N	NUMBER	UNIT	COST/UNIT	TOTAL
441-7011 441-7012	Curb Cut Wheelchair Ramp, Type A Curb Cut Wheelchair Ramp, Type B		+	0	EA EA	\$2,800.00 \$3,700.00	\$8,400. \$0.
441-7013 441-7014	Curb Cut Wheelchair Ramp, Type C Curb Cut Wheelchair Ramp, Type D			0 12	EA EA	\$2,400.00 \$2,000.00	\$0 \$24,000
999-5200	DETECTABLE WARNING SURFACE	warning surface 8 SF/ea (24"x48" typ.)		96	SF	\$2,000.00	\$3,360
UNDEF	Boardwalk with railing	Two boardwalks sections #1 sta 108+18 (148 LF) and #2 sta. 87+40 (62 LF)		210	LF	\$2,250.00 Subtotal	\$472,500 \$508,260
	Signage on side path						
636-6025 636-6026	Bicycles only Wayfinding Signage		+	4	EA EA	\$1,000.00 \$1,000.00	\$4,000 \$0
636-6027	Mile Markers			0	EA	\$1,000.00	\$0
652-9002	Yellow Centerline			0	LF	\$3.00 Subtotal	\$0 \$4,000
	Roadway						
441-0018	Concrete Driveway Apron (8" Thick)	13 driveways 20' x 25' typ. =500 sf typ. X 13 + 6500 sf/9 = 722 sy		722	SY	\$125.00	\$90,250
441-6222	Concrete Curb and Gutter 8" x 30" Tp 2	(Rivercrest Drive, Roberts Landing Cove & Lexington Dr.)10 radius at 35' LF each typ. + road diet ((sta 67+60 to 64+10)		1400	LF	\$30.00	\$42,000
402-3103	Recycled Asph Conc 9.5 MM Superpave, Type II, GP 2 Only, Incl Bituminous Matl & H Lime(2")	22 ft wide x 550 lf* = 12100 sf(1344sy)(Sta 67+62 to 64+10) + transition		156	TN	\$205.00	\$31,980
402-3130	Recycled Asph Conc 12.5 MM Superpave, GP 2 Only, Incl Matl & H Lime (1.5")	22 ft wide x 550 lf = 12100 sf(1344sy)		117	TN	\$200.00	\$23,400
432-5010	MILL ASPH CONC PVMT, VARIABLE DEPTH			0	SY	\$20.00	\$0
	Signing and Marking						
UNDEF	Stop Sign Multiple traffic signs	roadway		10	EA EA	\$1,200.00 \$1,200.00	\$7,200 \$12,000
652-5701	Stop Bar	6 at 12' (drives)		60	LF	\$6.00	\$360
652-9002 UNDEF	Yellow Centerline	sht 5 road diet (sta 67+62 to 64+10) = 350 lf + transition		550 0	LF EA	\$3.00 \$25,000.00	\$1,650 \$0
441-0748	Rectangular Rapid Flashing Beacon Concrete Median 6"	Mid Block Crossing, none in Ph2 Phase 1 only		0	EST	\$20,000.00	\$0
						Subtotal	\$208,840
670-9710	Utilities Relocate Fire Hydrant			3	EA	\$3,500.00	\$10,500
668-4300	Proposed Storm Manhole		#	0	EA	\$2,500.00	\$0
668-2100 550-1181	Drop Inlet GP1 GDOT 1019A Storm Pipe 18" RCP			0	EA LF	\$3,500.00 \$75.00	\$0 \$0
550-1240	Storm Pipe 24" RCP			0	LF	\$100.00	\$(
UNDEF	Concrete End Section			0	EA	\$5,000.00	\$0
603-1024	Rip Rap			0	TN	\$85.00 Subtotal	\$0,500 \$10,500
	Site Amenities					Subtotui	910,500
643-8300	Furnishings Bollard	1 at each corner at trail entries		15	EA	\$1,500.00	\$22,500
754-5000	Bench	2 at intersection with Roswell Road		2	EA	\$4,000.00	\$8,000
UNDEF UNDEF	Trash Receptacle Traffic Control Signage	1 at intersection with Roswell Road at midblock crossing locations	+	1 0	EA EA	\$2,500.00 \$500.00	\$2,500 \$0
641-2200	Cable Handrailing (turndown slabs only)	42" Cable Handrailing		289	LF	\$200.00	\$57,800
UNDEF	Picket Fencing (walls > 6' ht)	48" ht top of walls >6' ht		927	LF	\$125.00 Subtotal	\$115,875 \$206,675
	Landscape					Subtotal	3200,073
708-1000	Plant Topsoil	4" for sod areas + plants		300	CY	\$50.00	\$15,000
702-9025	Landscape Mulch	4" depth	-	1060	SY	\$20.00	\$21,200
700-9300	Sod	bermuda		1602	SY	\$15.00	\$24,030
UNDEF	Planting Street Tree	45 gal., 3" cal., variety TBD		36	EA	\$1,500.00	\$54,000
UNDEF	shrubs (3' o.c.)	3 gal, variety TBD	-	1552	EA	\$40.00	\$62,080
						Subtotal	\$176,310
	+						
		O&P	P/Proj	Mgmt/Mo	bilization/Bo	Project Subtotal nding & Insurance (25%)	\$5,505,562 \$1,376,390
***************************************		OSP	P/Proj			Project Subtotal nding & Insurance (25%) 20 % Contingency	\$176,310. \$5,505,562. \$1,376,390. \$1,101,112. \$660.667.
		OSP	P/Proj			Project Subtotal nding & Insurance (25%)	\$5,505,562. \$1,376,390. \$1,101,112. \$660,667.
	Items Outside Construction Contact	OSP	P/Proj			Project Subtotal Inding & Insurance (25%) 20 % Contingency Iginieering Services (12%)	\$5,505,562. \$1,376,390 \$1,101,112
	items Outside Construction Contract Utility Pole Relocations and Pedestrian Lighting		P/Proj		Design & Eng	Project Subtotal Inding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total	\$5,505,562. \$1,376,390 \$1,101,112 \$660,667 \$8,643,733.
441-0104	Rems Outside Construction Contract Utility Pole Relocations and Pedestrian Ughting Relocate Utility Fole	12 relocated	P/Proj			Project Subtotal Inding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$25,000.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733.
441-0104	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate light pole, add utility pole	12 relocated 3 added power poles, 1 relocate light	P/Proj	12	Design & Eng EA EA	Project Subtotal Inding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$25,000.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733.
	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole	12 relocated	P/Proj	12	Design & Eng	Project Subtotal nding & Insurance (25%) 20 % Contingency ginleering Services (12%) Total \$25,000.00 \$10,000.00 \$12,000.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733. \$300,000 \$40,000 \$960,000
441-0104 441-0105 UNDEF 682-9950	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Light pole, add utility pole Add Pedestrian Light Pole Directional Bore	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE	P/Proj	12 4 80 1 100	Design & Eng EA EA EA EA EA LF	Project Subtotal nding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$25,000.00 \$10,000.00 \$12,000.00 \$15,000.00 \$220.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733. \$300,000 \$40,000 \$960,000 \$15,000
441-0104 441-0105 UNDEF 682-9950 682-6219	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduit NOMMETL, TP 2, 2 IN and Elect. Wire	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE	P/Proj	12 4 80 1	Design & Eng EA EA EA EA	Project Subtotal anding & Insurance (25%) 20 % Contingency inieering Services (12%) Total \$\$ \$25,000.00 \$\$ 10,000.00 \$\$ 12,000.00 \$\$ 220.00 \$\$ 520.00 \$\$ 515	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733. \$300,000 \$40,000 \$960,000 \$15,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduin NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THM AWK NO TBD	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500	Design & Eng EA EA EA EA LF LF EA LF	Project Subtotal adding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$10,000.00 \$10,000.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733. \$300,000 \$40,000 \$15,000 \$2,000 \$27,500 \$20,000 \$55,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500	Utility Pole Relocations and Pedestrian Ughting Relocate Utility Pole Relocate Ught pole, add utility pole Add Pedestrian Light Pole Directional Bore Condun NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE	P/Proj	12 4 80 1 100 6500 2	Design & Eng EA EA EA EA LF LF EA	Project Subtotal Inding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$\$ \$25,000.00 \$\$12,000.00 \$\$12,000.00 \$\$15,000.	\$5,505,562 \$1,376,390 \$1,101,112 \$660,667 \$8,643,733. \$300,000 \$40,000 \$15,000 \$2,000 \$27,500 \$20,000 \$55,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduin NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THM AWK NO TBD	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500	Design & Eng EA EA EA EA LF LF EA LF	Project Subtotal anding & Insurance (25%) 20 % Conflict (25%) 20 % Conflict (25%) 20 % Conflict (25%) Total 525,000.00 \$10,000.00 \$15,000.00 \$1	\$5,505,562 \$1,376,390 \$1,101,112 \$660,655 \$8,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$2,000 \$20,000 \$65,000 \$65,000 \$65,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduin NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THM AWK NO TBD	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500	Design & Eng EA EA EA EA LF LF EA LF	Project Subtotal adding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$10,000.00 \$10,000.00	\$5,505,562 \$1,376,390 \$1,101,112 \$660,655 \$8,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$2,000 \$20,000 \$65,000 \$65,000 \$65,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THW AWK NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0	EA EA EA LF EA LF EA EA	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00	\$5,505,562 \$1,376,396 \$1,1376,396 \$60,666 \$8,643,733. \$300,000 \$40,000 \$15,000 \$15,000 \$20,000 \$55,500
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NONMETI, TP 2, 21N and Elect. Wire Electrical Service Point Cable, TP THW AWG NO TBD Relocate Electrical Box Right-OF-Way and Easement Cost	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500	Design & Eng EA EA EA EA LF LF EA LF	Project Subtotal anding & Insurance (25%) 20 % Conflict (25%) 20 % Conflict (25%) 20 % Conflict (25%) Total 525,000.00 \$10,000.00 \$15,000.00 \$1	\$5,505,562 \$1,376,3905,562 \$1,101,1137,5905,560,560 \$8,643,733. \$300,000 \$40,000 \$15,000 \$15,000 \$20,000 \$20,000 \$55,0
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THW AWK NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0	EA EA EA LF EA LF EA EA	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00 \$10,000.00	\$5,505,562 \$1,376,390 \$660,667 \$8,643,733, \$300,000 \$40,000 \$560,000 \$2,000 \$2,000 \$37,000 \$57,000 \$55
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 1744 AWAN OT BD Relocate Electrical Box Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential Multi-Family Residential	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0	Design & Eng EA EA EA EA LF EA LF EA SF SF	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$515,000.00 \$515,000.00 \$515,000.00 \$515,000.00 \$515,000.00 \$515,000.00 \$515,000.00	\$5,505,562 \$1,376,396 \$660,661 \$8,643,733 \$300,000 \$40,000 \$500,000 \$2,000 \$2,000 \$52,000 \$52,000 \$53,000 \$54,000 \$55,000 \$5
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THM AWG NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Industrial Single Family Residential	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0	Design & English	Project Subtotal anding & Insurance (25%) 20 % Confine	\$5,005,562 \$1.376,396 \$60,660,660,660,660,660,660,660,660,660,
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NONMETI, TP 2, 21N and Elect. Wire Electrical Service Point Cable, TP THW AWG NO TBD Relocate Electrical Box Right-OF-Way and Easement Cost Right-OF-Way and Easement Cost Right-OF-Way Residential Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Condominiums Multi-Family Residential Multi-Family Residential - Townhomes	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 1 100 6500 2 6500 0	EA EA EA EA EA EA EA SF SF SF	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 30 % Confliction 30 % Confliction 310,000 00 510,0	\$5,005,562 \$1.376,396 \$60,660,660,660,660,660,660,660,660,660,
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduin NOMMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THW AWG NOT BID Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Lindustrial Single Family Residential Multi-Family Residential Condominiums	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 1 100 6500 2 6500 0	Design & English EA	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$515,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$55,000.00 \$50,000.00 \$50,000.00 \$50,000.00 \$50,000.00 \$50,000.00 \$50,000.00 \$50,000.00 \$50,000.00	\$5,505,562 \$1,376,396 \$660,661 \$8,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$2,000 \$52,000 \$52,000 \$53,000 \$54,000 \$55,000 \$
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 1144 WANG NO TBD Relocate Electrical Box Relocate Electrical Box Rejot-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential—Condominums Industrial Single Family Residential Single Family Residential	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0 0 6540 0 0 0	Design & Engel	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$15,000.00 \$15,000.00 \$15,000.00 \$515,000.00 \$55,0	\$5,505,562 \$1,376,396 \$660,661 \$8,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$2,000 \$2,000 \$5
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NOMMETI, TP 2, 21N and Elect. Wire Electrical Service Point Cable, TP THW AWG NO TBD Relocate Electrical Box Right-Of-Way And Essement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Permanent Essement (50% of ROW) Industrial Single Family Residential Industrial Single Family Residential Industrial Single Family Residential Multi-Family Residential Single Family Residential Single Family Residential Multi-Family Residential Single Family Residential Single Family Residential Multi-Family Residential Multi-Family Residential Single Family Residential Multi-Family Residential	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 2 6500 0 0 0 6500 0 0	Design & English EA EA EA EA EA EA EF EA E	Project Subtotal inding & Insurance (25%) 20 % Contingent 20 % Contingent 30 % Contingent 30 % Contingent 31,000 00 \$11,000 00 \$11,000 00 \$12,000 00 \$13,000 00 \$13,000 00 \$13,000 00 \$13,000 00 \$13,000 00 \$13,000 00 \$31,0	\$5,505,562 \$1,376,396 \$660,667 \$8,643,733 \$300,000 \$40,000 \$300,000 \$300,000 \$300,000 \$500,00
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 1144 WANG NO TBD Relocate Electrical Box Relocate Electrical Box Rejot-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential—Condominums Industrial Single Family Residential Single Family Residential	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	12 4 80 1 100 6500 0 0 0 0 0 0 0 0	Design & English EA EF EA EF EA EF EA EF EA EF	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$13,000.00 \$10,000 \$10	\$5,505,562 \$1,376,396 \$1,310,111,111 \$660,6616 \$6,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$5,000 \$
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NOMMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 114M WAR NO TBD Relocate Electrical Box Relocate Electrical Box Rejot-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Residential Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Condominiums Multi-Family Residential - Condominiums Multi-Family Residential - Condominiums Multi-Family Residential - Condominiums	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	122 4 4 80 80 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Design & Engels EA EA EA EA EA EA EA E	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 525,000.00 530,000.00 530,000.00 532,000 530,000.00 535,000	\$5,505,562 \$1,376,396 \$1,310,111,111 \$660,6616 \$6,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$5,000 \$
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condun NOMMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP THW AWK NO TBD Relocate Electrical Box Relocate Electrical Box Rejot-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Temp Construction Easement (20% of ROW)	12 relocated 3 added power poles, 1 relocate light 13* DECORATIVE 18* DECORATIVE GATEWAY To be determined	P/Proj	122 4 4 80 0 1 1 100 0 0 0 0 0 0 0 0 0 0 0 0 0	EA E	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 310,000.00 \$10,000.00 \$12,000.00 \$12,000.00 \$10,00	\$5,505,562 \$1,376,396 \$1,310,11,111 \$5660,6606,660,660,660,660,660,660,660,66
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condus NOMMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 114M WAN ON TBD Relocate Electrical Box Relocate Electrical Box Rejot-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes	12 relocated 3 added power poles, 1 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY	P/Proj	122 4 4 80 80 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Design & Engels EA EA EA EA EA EA EA E	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 525,000.00 530,000.00 530,000.00 532,000 530,000.00 535,000	\$5,505,562 \$1,376,396 \$1,310,11,111 \$5660,6606,660,660,660,660,660,660,660,66
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NONMETI, 19 2, 21N and Elect. Wire Electrical Service Point Cable, 17 THW AWG NO TBD Relocate Electrical Box Right-Of-Way Cost Industrial Single Family Residential Agartments Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Re	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE	P/Proj	12 4 80 80 6500 0 0 0 0 0 0 0 0 0 0 0 3 2 2 0 0 0 0 0	EA E	Project Subtotal inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction \$525,000.00 \$10,000.00 \$10,000.00 \$12,000.00 \$13,000.00 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000 \$10,000	\$5,505,562 \$1,376,396 \$1,310,11,11 \$560,661 \$8,643,733 \$300,000 \$40,000 \$54
141-0104 141-0105 JNDEF 582-9950 582-6219 582-8500 582-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condux NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, TP 1744 AWG NO TBD Relocate Electrical Box Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial Single Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial	12 relocated 3 added power poles, 1 relocate light 13* DECORATIVE 18* DECORATIVE GATEWAY To be determined	P/Proj	122 4 4 80 0 1 1 100 0 0 0 0 0 0 0 0 0 0 0 0 0	EAA	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction 310,000.00 510,000.00 512,000.00 512,000.00 515,00	\$5,505,562 \$1,376,396 \$1,310,111; \$660,661 \$6,643,733. \$300,000 \$40,000 \$960,000 \$5,00
141-0104 141-0105 JNDEF 582-9950 582-6219 582-8500 582-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Relocate Relocate Legistrian Service Polit Carble, 17 FTHW AWG NO TBD Relocate Electrical Box Relocate Electrical Box Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Individual Single Family Residential Single Family Residential Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential-Condominiums Multi-Family Residential-Condominiums Nutri-Family Residential-Condominiums Nutri-Family Residential-Apartments Nutri-Family Residential-Apartments Nutri-Family Residential-Apartments	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE	P/Proj	12 4 80 80 6500 0 0 0 0 0 0 0 0 0 0 0 3 2 2 0 0 0 0 0	EAA EAA LF EAA L	Project Subtotal inding & Insurance (25%) 20 % Conting Continue (25%) 20 % Continue (25%) 20 % Continue (25%) 31,000,00 \$10,000,00 \$11,000,00 \$	\$5,505,562 \$1,376,396 \$1,310,117 \$560,667 \$8,643,733 \$300,000 \$40,000 \$40,000 \$300,0
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Light Pole Directional Bore Conduit NONMETI, 1P 2, 21N and Elect. Wire Electrical Service Point Cable, 1P THW AWG NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Condominiums	12 relocated 3 added power poles, 1 relocate light 13" DECORATIVE 18" DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5" wide & 10" wide	P/Proj	122 4 8 80 1 1 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	EA.	Project Subtotal inding & Insurance (25%) 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$11,000.00 \$12,000.00 \$13,000.00 \$	\$5,505,562 \$1,376,396 \$1,310,111.11 \$660,666 \$8,643,733 \$300,000 \$40,0
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Relocate Relocate Legistrian Service Polit Carble, 17 FTHW AWG NO TBD Relocate Electrical Box Relocate Electrical Box Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Individual Single Family Residential Single Family Residential Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential-Condominiums Multi-Family Residential-Condominiums Nutri-Family Residential-Condominiums Nutri-Family Residential-Apartments Nutri-Family Residential-Apartments Nutri-Family Residential-Apartments	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE	P/Proj	12 4 80 80 6500 0 0 0 0 0 0 0 0 0 0 0 3 2 2 0 0 0 0 0	EAA EAA LF EAA L	Project Subtotal inding & Insurance (25%) 20 % Conting Continue (25%) 20 % Continue (25%) 20 % Continue (25%) 31,000,00 \$10,000,00 \$11,000,00 \$	\$5,505,562 \$1,376,396 \$1,310,111.11 \$660,666 \$8,643,733 \$300,000 \$40,0
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Light Pole Directional Bore Conduit NONMETI, 1P 2, 21N and Elect. Wire Electrical Service Point Cable, 1P THW AWG NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Condominiums	12 relocated 3 added power poles, 1 relocate light 13" DECORATIVE 18" DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5" wide & 10" wide	P/Proj	122 4 8 80 1 1 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	EA.	Project Subtotal inding & Insurance (25%) 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$11,000.00 \$12,000.00 \$13,000.00 \$	\$5,505,562 \$1,376,396 \$1,310,111.11 \$660,666 \$8,643,733 \$300,000 \$40,0
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Light Pole Directional Bore Conduit NONMETI, 1P 2, 21N and Elect. Wire Electrical Service Point Cable, 1P THW AWG NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Apartments Multi-Family Residential - Condominiums	12 relocated 3 added power poles, 1 relocate light 13" DECORATIVE 18" DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5" wide & 10" wide	P/Proj	122 4 8 80 1 1 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	EA.	Project Subtotal inding & Insurance (25%) 20 % Confliction 30 % Confliction 525,000.00 \$10,000.00 \$11,000.00 \$12,000.00 \$13,000.00 \$	\$5,505,562 \$1,376,396 \$1,310,11,11 \$660,667 \$8,643,733 \$300,000 \$40,000 \$540,000 \$52,000 \$52,000 \$53,000 \$53,000 \$540,000 \$55,
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduct NONMETL, TP 2, 2N and Elect. Wire Electrical Service Point Cable, TP THW AWG NO TBD Relocate Electrical Box Right-Of-Way and Easement Cost Right-Of-Way Cost Industrial Single Family Residential Multi-Family Residential-Apartments Multi-Family Residential-Condominums Multi-Family Residential-Condominums Multi-Family Residential Single Family Residential Multi-Family Residential Multi-Family Residential Multi-Family Residential Multi-Family Residential Multi-Family Residential Multi-Family Residential Condominums Multi-Family Residential - Townhomes Temp Construction Easement (20% of ROW) Industrial Single Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Townhomes Multi-Family Residential - Condominiums	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5' wide & 10' wide	P/Proj	122 4 4 80 1 1 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	EA E	Protect Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction \$525,000.00 \$10,000.00 \$11,000.00 \$12,000.00 \$13,0	\$5,505,562 \$1,376,396 \$51,010,1121 \$560,667 \$8,643,733. \$300,0000 \$540,000
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condun NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, Pr Hrw AWC NO TBD Relocate Electrical Box Right-Of-Way Cost Industrial Single Family Residential Apartments Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Condominiums Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial Single Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Multi-Family Residential-Townhomes Public Property Driveway Easement	12 relocated 3 added power poles, 1 relocate light 13* OECORATIVE 18* DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5' wide & 10' wide parcel number unknown	P/Proj	12 4 80 1 100 6500 0 0 0 6540 0 0 0 0 0 347 15542	EA	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction \$1,000,000 \$10,000,000 \$12,000,000 \$12,000,000 \$15,000,00	\$5,505,562 \$1,376,390 \$1,301,112 \$560,667 \$8,643,733. \$300,000 \$40,000 \$40,000 \$540,000 \$52,000 \$52,000 \$53,00
141-0104 141-0105 JNDEF 582-9950 582-6219 582-8500 582-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Security Polity Relocate Security Polity Relocate Security Polity Relocate Security Polity Relocate Security Rel	12 relocated 3 added power poles, 1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined 06 0366 LL0295 - COMMERCIAL USE 5' wide & 10' wide	P/Proj	122 4 4 80 1 1 100 6500 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	EA E	Protect Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction \$525,000.00 \$10,000.00 \$11,000.00 \$12,000.00 \$13,0	\$5,505,562 \$1,376,396 \$1,310,111 \$5660,667 \$8,643,733 \$300,000 \$40,000 \$540
441-0104 441-0105 UNDEF 682-9950 682-6219 682-8500 682-1303	Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condun NONMETL, TP 2, 2 IN and Elect. Wire Electrical Service Point Cable, Pr Hrw AWC NO TBD Relocate Electrical Box Right-Of-Way Cost Industrial Single Family Residential Apartments Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Condominiums Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Permanent Easement (50% of ROW) Industrial Single Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial Single Family Residential-Townhomes Temp Construction Easement (20% of ROW) Industrial Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Apartments Multi-Family Residential-Condominiums Multi-Family Residential-Townhomes Multi-Family Residential-Townhomes Public Property Driveway Easement	12 relocated 3 added power poles, 1 relocate light 13" DECORATIVE 18" DECORATIVE GATEWAY To be determined To be determined 06 0366 LL0295 - COMMERCIAL USE 5' wide & 10' wide parcel number unknown PANDS 03660 LL0251(CRNRA)	P/Proj	12 4 80 1 100 6500 0 0 0 6540 0 0 0 0 0 347 15542	EA	Project Subtotal Inding & Insurance (25%) 20 % Confliction 20 % Confliction 30 % Confliction \$525,000.00 \$10,000.00 \$10,000.00 \$12,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$13,000.00 \$10,000 \$	\$5,505,562 \$1,376,396 \$1,310,111; \$660,661 \$8,643,733. \$300,000 \$40,000 \$960,000 \$2,000 \$52,000 \$52,000 \$53,000 \$540,000 \$550,000

07.4 COST ESTIMATE

PHASE 2 PREFERRED, SHEET 1 OF 2

CONCEPTUAL COST ESTIMATE

ROBERTS DRIVE - Phase 2 - PREFERED

Roswell Road to end of Phase 1

Station 118+95 to Station 53+10 = (6,585 LF = 1.2 miles)

	ITEM			NUMBER	UNIT	COST/UNIT	TOTAL
150 1000	Demolition and Grading Items	Notes			1.0	Ć150.000.00	Ć4 F0 000 0
150-1000 210-0100	Traffic Control Grading Complete		-	1 1	LS LS		\$150,000.0 \$1,250,000.0
201-1500	Clearing and Grubbing			1.5	AC		\$2,250.0
610-0300	Remove wood fence in R/W	sta 105+00 to 103+28 (exclude drive)		107	LF		\$2,675.0
UNDEF	Remove Ex. Tree	sta 86+55 and sta 81+25 (Lexington Dr. entry)		2	EA		\$2,000.0
510-9097	Remove sign wall	Summer Crossing (sta 54+60) 6 brick columns, 2 brick walls, 2 wood signs		2	EA	\$10,000.00	\$20,000.0
610-2815	Remove existing Sidewalk	Rivermere Way and Roberts Landing Cove		4155	SY	\$6.50	\$27,007.5
010-2013	Nemove existing Sidewalk	2 at The Lodge at Rivercrest Dr., 4 block columns stone veneer, lighted(sta 84+50 and	_	4133	31	Ş0.30	J27,007.J
		85+00) + 2 at Roberts Landing Cove, 4 brick columns aluminum picket fencing, brick					
		knee wall(sta 83+00-84+00) + 2 at Lexington Dr.,5 brick columns, wood fence,					
610-9097	Remove sign walls and fence in R/W	aluminum sign (sta 79+50 - 81+00)	_	6	EA	\$10,000.00	\$60,000.0
610-0355	Remove entrance curb & gutter	Rivercrest Drive, Roberts Landing Cove & Lexington Dr.		300	LF	\$6.50	\$1,950.0
611-8050	ADJUST MANHOLE TO GRADE			3	EA	\$2,000.00	\$6,000.0
	· · · · · ·					Subtotal	\$1,521,882.5
	Erosion and Sediment Control						
163-0232	Temporary grassing	10 sf x length		1.5	AC		\$900.0
163-0300	Construction exit			2	EA		\$4,000.0
163-0529	Construct and Remove temporary sediment barrier or baled straw check dam			500	LF		\$3,500.0
163-0550	Maintain Inlet Sediment Trap		_	7	EA		\$1,575.0
165-0010	Maintenance of Temp Silt Fence -			6500	LF	\$3.00	\$19,500.0
165-0101	Maintenance of Construction Exit		_	2	EA		\$1,500.0
165-0101	Inlet Sediment Trap Maintenance		-	7	EA		\$1,050.0
171-0030	Temporary Silt Fence, Type C	For	+	6500	LF		\$32,500.0
455-1000	Filter Frabic for Embankment Stablization	EST.	+	1000	SY		\$6,000.0
603-2182	Stn Dumped Rip Rap, TP, 3, 24" Restrict Enne (Oceans) 4th		+	100	SY		\$7,000.0
643-8200	Barrier Fence (Orange), 4ft		+	6500	LF		\$19,500.0
700-6910	Permanent grassing			1.5	AC		\$1,875.0
	Cide Debband Courses Wells					Subtotal	\$98,900.0
	Side Path and Concrete Walk						
441-0104	Conc. Sidewalk 4 IN	4" under pavers only		67	SY	\$65.00	\$4,355.0
			+				
	Brick pavers	Roswell Rd. entry plaza		602	SF	\$15.00	\$9,030.0
				7			
441-0106	Conc. Sidewalk 6 IN			5970	SY	\$95.00	\$567,150.0
310-5040	CD ACCD DACE CDC A INCL. INC. AAATI			5970	SY	£35.00	£200.0E0.0
310-5040	GR AGGR BASE CRS, 4 INCH, INCL MATL			5970	51	\$35.00	\$208,950.0
						Subtotal	\$789,485.0
	Walls						
500-3120	Wall #1 Turnup Concrete Slab with Reinforcement	sta, 117+98' to 116+67 (131 lf, avg.2-3' hgt. x 8" wide)		10	CY	\$1,550.00	\$15,500.0
607-3001	Wall #1 Stone Cap	12" x 24" cap		140	LF		\$4,900.0
608-3000	Wall #1 Stone Veneer	granite rubble, ashlar pattern		393	SF		\$13,755.0
500-3120	Wall #2 Turnup Concrete Slab with Reinforcement	sta, 113+55 to 112+74 (81 lf, 2'avg.hgt x 8" wide)		4	CY		\$6,200.0
607-3001	Wall #2 Stone Cap	12" x 24" cap		85	LF		\$2,975.0
608-3000	Wall #2 Stone Veneer	granite rubble, ashlar pattern		162	SF		\$5,670.0
500-3120	Wall #3 Turnup Concrete Slab with Reinforcement	sta. 111+77 to sta. 110+85 (92 lf, 2' avg hgt. x 8" wide)		5	CY	\$1,550.00	\$7,750.0
607-3001	Wall #3 Stone Cap	12" x 24" cap		95	LF	\$35.00	\$3,325.0
608-3000	Wall #3 Stone Veneer	granite rubble, ashlar pattern		184	SF	\$35.00	\$6,440.0
500-3120	Wall #4 Conc. Gravity Walls GDOT 9031L w/picket fence	sta. 110+62 to sta. 109+49 (113 lf, 6' avg. hgt.x avg.2.5'wide)		73	CY		\$102,200.0
607-3001	Wall #4 Stone Cap	12" x 24" cap		120	LF		\$4,200.0
608-3000	Wall #4 Stone Veneer	granite rubble, ashlar pattern		680	SF		\$23,800.0
500-3120	Wall #5 Turnup Concrete Slab with Reinforcement	sta. 105+80 to sta.104+74 (106 lf, 3' avg. hgt.x8" wide)		10	CY		\$15,500.0
607-3001	Wall #5 Stone Cap	12" x 24" cap		110	LF		\$3,850.0
608-3000	Wall #5 Stone Veneer	granite rubble, ashlar pattern		318	SF	\$35.00	\$11,130.0
500-3120	Wall #6 Conc. Gravity Walls GDOT 9031L	sta. 104+40 to sta. 103+23 (117 lf, 4' avg. hgt.x 2.5' wide)		54	CY	\$1,400.00	\$75,600.0
607-3001	Wall #6 Stone Cap	12" x 24" cap	\perp	55	LF		\$1,925.0
608-3000	Wall #6 Stone Veneer	granite rubble, ashlar pattern	\perp	470	SF		\$16,450.0
500-3120	Wall #7 Conc. Gravity Walls GDOT 9031L w/picket fence	sta. 102+72 to sta. 97+20 (552 lf, 6-7' avg. hgt. x 2.5' wide)	ፗ	410	CY		\$574,000.0
607-3001	Wall #7 Stone Cap	12" x 24" cap	I	552	LF		\$19,320.0
608-3000	Wall #7 Stone Veneer	granite rubble, ashlar pattern		3600	SF		\$126,000.0
500-3120	Wall #8 Conc. Gravity Walls GDOT 9031L	sta. 96+97 to sta. 95+78 (119 lf, 4' avg. hgt.x 2.5' wide)		55	CY		\$77,000.0
	Wall #8 Stone Cap	12" x 24" cap	_	119	LF		\$4,165.0
607-3001		granite rubble, ashlar pattern	- 1	480	SF		\$16,800.0
607-3001 608-3000	Wall #8 Stone Veneer		-				
607-3001 608-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)		150	CY	\$1,400.00	
607-3001 608-3000 500-3120 607-3001	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap		150 265	LF	\$35.00	\$9,275.0
607-3001 608-3000 500-3120 607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cyener Wall #9 Stone Veneer	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern		150 265 1300	LF SF	\$35.00 \$35.00	\$9,275.0 \$45,500.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L w/picket fence	sta. 94+37 to sta. 91+71 (266 if, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 if, 5-6'avg. hgt.x 2.5'wide)		150 265 1300 60	LF SF CY	\$35.00 \$35.00 \$1,400.00	\$9,275.0 \$45,500.0 \$84,000.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #3 Stone Veneer Wall #3 Conc. Gravity Walls GDOT 9031L w/picket fence Wall #10 Stone Cap	\$1a. 944-37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 88-34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap		150 265 1300 60 100	LF SF CY LF	\$35.00 \$35.00 \$1,400.00 \$35.00	\$210,000.0 \$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L w/picket fence Wall #10 Stone Cap Wall #10 Stone Veneer	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern		150 265 1300 60 100 550	LF SF CY LF SF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L w/picket fence Wall #10 Stone Cap Wall #10 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L w/picket fence Wall #11 Conc. Gravity Walls GDOT 9031L w/picket fence	Sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf,5-6' avg. hgt.x 2.5' wide)		150 265 1300 60 100 550 70	LF SF CY LF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$98,000.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001	Wall #9 Conc. Gravity Walls GDOT 9031L	Sta. 944-37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 884-34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-73 to sta. 85-75 (103 lf,5-6' avg. hgt.x 2.5' wide) 12" x 24" cap		150 265 1300 60 100 550 70 100	LF SF CY LF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$98,000.0 \$3,500.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 9031L Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L Wall #10 Stone Cap Wall #10 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Cap Wall #11 Stone Cap Wall #11 Stone Veneer Wall #11 Stone Veneer Wall #13 Stone Veneer Wall #13 Stone Veneer Wall #13 Stone Veneer Wall #13 Stone Veneer	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf,5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf,5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern		150 265 1300 60 100 550 70 100 600	LF SF CY LF SF CY LF SF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$98,000.0 \$3,500.0 \$21,000.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 9031L	Sta. 944-37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 884-34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5'wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-73 to sta. 85-75 (103 lf,5-6' avg. hgt.x 2.5' wide) 12" x 24" cap		150 265 1300 60 100 550 70 100	LF SF CY LF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$98,000.0 \$3,500.0 \$21,000.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L	\$1a. 944-37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5'wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 86-78 to sta. 85-75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-78 to sta. 85-75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide)		150 265 1300 60 100 550 70 100 600	LF SF CY LF SF CY LF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$1,500.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$98,000.0 \$21,000.0 \$10,850.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 500-3120 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L	\$1a. 94+37 to sta. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88-34 to sta. 87-40 (94 If, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-78 to sta. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84-62 to 83+75 (87 If, 2-3' avg. ht x 8" wide) sta. 65+59 to 62+47 (312 If, 6' avg hgt.x 12" wide)		150 265 1300 60 100 550 70 100 600 7	LF SF CY LF SF CY CY CY CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$1,500.00	\$9,275.6 \$45,500.0 \$84,000.0 \$3,500.6 \$19,250.0 \$98,000.0 \$3,500.0 \$10,850.0 \$10,850.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 500-3120 500-3120 500-3120 500-3120	Wall #9 Conc. Gravity Walls GDOT 90311.	\$\text{s1.94+37 to \$ta.91+71 (266 \text{ if, 5' avg. hgt.x 2.5' wide)}\$ 12" x 24" cap graniter rubble, ashlar pattern \$ta. 88+34 to \$ta. 87+40 (94 \text{ if, 5-6'avg. hgt.x 2.5' wide)}\$ 12" x 24" cap graniter rubble, ashlar pattern \$ta. 86+78 to \$ta. 85+75 (103 \text{ if, 5-6' avg. hgt.x 2.5' wide)}\$ 12" x 24" (ap) graniter rubble, ashlar pattern \$ta. 84+62 to 83+75 (87 \text{ if, 2-3' avg. ht x 8" wide)}\$ \$ta. 84+62 to 83+75 (87 \text{ if, 2-3' avg. ht x 8" wide)}\$ \$ta. 65+59 to 62+47 (312 \text{ if, 6' avg hgt.x 12" wide)}\$ \$ta. 65+59 to 62+47 (312 \text{ if, 6' avg hgt.x 12" wide)}\$		150 265 1300 60 100 550 70 100 600 7	LF SF CY LF SF CY CY LF CY LF SF CY LF LF SF CY LF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,550.00	\$9,275.6 \$45,500.0 \$84,000.6 \$3,500.0 \$19,250.0 \$98,000.0 \$3,500.0 \$21,000.0 \$10,850.0 \$10,950.0 \$10,950.0
507-3001 508-3000 508-3000 507-3001 508-3000 507-3001 508-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L	\$1a. 944-97 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide) sta. 65+59 to 62+47 (312 lf, 6' avg hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern		150 265 1300 600 100 550 70 100 600 7	LF SF CY LF SF CY LF SF CY LF SF CY LF SF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$9.275.(\$44,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$3,500.0 \$21,000.0 \$10,850.0 \$120,000.0 \$10,920.0 \$65,520.0
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 90311.	\$1a. 944-37 to \$1a. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 88-34 to \$1a. 87-40 (94 If, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86-78 to \$1a. 85-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86-78 to \$1a. 85-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+62 to 83-75 (87 If, 2-3' avg. ht x 8" wide) \$1a. 86-55 to 62-47 (312 If, 6' avg hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 62-47 to 61-85 (62 If, 6' avg hgt.x 8" wide)		150 265 1300 60 100 550 70 100 600 7	LF SF CY LF SF CY LF SF CY LF SF CY CY CY CY CY CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$1,550.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$9.275.(\$45,500.0 \$84,000.0 \$19,250.0 \$19,250.0 \$21,000.0 \$10,850.0 \$10,920.0 \$65,520.0 \$16,500.0
507-3001 508-3000 508-3000 508-3000 500-3120 507-3001 508-3000 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120 500-3120	Wall #9 Conc. Gravity Walls GDOT 90311 Wall #9 Stone Cap Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 9031L w/picket fence Wall #10 Stone Veneer Wall #11 Stone Veneer Wall #11 Stone Cap Wall #12 Stone Cap Wall #12 Tone Cap Wall #12 Tone Cap Wall #13 Stone Veneer Wall #14 Stone Veneer Wall #15 Stone Veneer Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Cap Wall #18 Stone Veneer Wall #18 Stone Veneer Wall #18 Stone Veneer Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Veneer Wall #18 Poured in Place Wall w/picket fence	sta. 944-37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern sta. 884-34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-75 to sta. 85-75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-75 to sta. 85-75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84-62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 62-47 to 61-85 (62 lf, 6' avg hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 62-47 to 61-85 (62 lf, 6' avg hgt.x 8" wide)		150 265 1300 60 100 550 70 100 600 7 80 312 1872 10 688	LF SF CY LF SF CY LF SF CY LF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,500.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$9,275,0 \$45,500,1 \$84,000,1 \$3,500,0 \$3,500,0 \$3,500,0 \$3,500,0 \$21,000,0 \$10,850,0 \$120,000,0 \$10,920,0 \$16,500,0 \$10,200,0 \$10,200,0 \$10,200,0
507-3001 508-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 9031L	\$1a. 944-37 to \$1a. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 884-34 to \$1a. 87+40 (94 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+78 to \$1a. 88+75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+78 to \$1a. 88+75 (87 If, 2-3' avg. htt x 8" wide) \$12" x 24" cap \$13" c 5+70 (215 If, 7.5' avg hgt.x 8" wide) \$15a. 65+85 to \$9+70 (215 If, 7.5' avg hgt.x 12" wide)		150 265 1300 60 100 550 70 100 600 7 80 312 1872 10 688 215	LF SF CY LF SF CY CY CY LF CY LF LF CY LF	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$1,500.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$9,275. \$45,500.1 \$84,000.1 \$3,500.1 \$19,250.1 \$3,500.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1 \$10,250.1
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 500-3120 500-3120 500-3120 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 608-3000	Wall #9 Conc. Gravity Walls GDOT 90311. Wall #9 Stone Cap	\$\text{st. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide)}\$ 12" x 24" cap graniter rubble, ashlar pattern \$\text{sta. 88+34 to sta. 87+40 (98 lf, 5-6'avg. hgt.x 2.5' wide)}\$ 12" x 24" cap graniter rubble, ashlar pattern \$\text{sta. 86+78 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide)}\$ 12" x 24" (cap graniter rubble, ashlar pattern \$\text{sta. 84+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide)}\$ \$\text{sta. 84+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide)}\$ \$\text{sta. 65+59 to 62+47 (312 lf, 6' avg hgt.x 12" wide)}\$ \$\text{12" x 24" cap}\$ granite rubble, ashlar pattern \$\text{sta. 62+47 to 61455 (62 lf, 6' avg. hgt.x 8' wide)}\$ \$\text{sta. 62+47 to 61455 (62 lf, 6' avg. hgt.x 8' wide)}\$ \$\text{sta. 64+85 to 59+70 (215 lf, 7.5' avg. hgt.x 12" wide)}\$ \$\text{12" x 24" cap}\$ granite rubble, ashlar pattern		150 265 1300 60 100 500 70 100 600 7 7 80 80 1872 1872 106 68 2155 1613	LF SF SF CY CY CY CY CY CY CY SF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.0 \$45,000.0 \$34,000.0 \$35,000.0 \$19,250.0 \$35,000.0 \$21,000.0 \$10,250.0 \$10,250.0 \$10,250.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0 \$10,200.0
607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 9031L	\$1a. 944-37 to sta. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) granter rubble, ashlar pattern sta. 88-34 to sta. 87-40 (94 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granter rubble, ashlar pattern sta. 86-78 to sta. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granter rubble, ashlar pattern sta. 86-78 to sta. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granter rubble, ashlar pattern sta. 65+59 to 62+47 (312 If, 6' avg hgt.x 12" wide) 12" x 24" cap granter rubble, ashlar pattern sta. 62+47 to 61+85 (62 If, 6' avg hgt.x 8" wide) sta. 63+85 to 59+70 (215 If, 5' avg hgt.x 2" wide) 2" x 24" cap granter rubble, ashlar pattern sta. 62+97 to 61+85 (62 If, 6' avg hgt.x 12" wide) 2" x 24" cap granter rubble, ashlar pattern sta. 62+97 to 61+85 (62 If, 6' avg hgt.x 8" wide)		150 265 265 500 100 100 100 100 600 7 8 8 8 8 1872 1872 1872 215 1613 3 3	LF SF CY CY CY LF SF CY CY LF SF CY CY CY LF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.6 \$45,500.1 \$84,000.0 \$84,000.0 \$3,500.1 \$98,000.0 \$19,250.0 \$10,950.0 \$10,950.0 \$10,950.0 \$15,500.
507-3001 508-3000	Wall #9 Conc. Gravity Walls GDOT 90311. Wall #9 Stone Cap Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 Stone Veneer Wall #3 I Stone Veneer Wall #3 Poured in Place Wall w/picket fence Wall #3 I Stone Veneer Wall #3 I Stone Veneer Wall #3 B Stone Veneer Wall #3 B Stone Veneer Wall #3 B Stone Veneer Wall #3 I Stone Veneer Wall #3 Stone Veneer Wall #3 I St	\$1a. 944-37 to \$1a. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 88+34 to \$1a. 87+40 (98 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+78 to \$1a. 85+75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+78 to \$1a. 85+75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86+62 to \$1a. 87+75 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1a. 62+47 to 61+55 (62 If, 6' avg hgt.x 12" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1a. 62+47 to 61+55 (62 If, 6' avg hgt.x 12" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1a. 92+70 to 59+32 (83 If x 2' avg hgt.x 12" wide)		150 1300 60 1000 5500 70 1000 600 7 80 312 1872 106 68 1613 3	LF SF SF CY CY LF SF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.6 \$45,000.6 \$84,000.1 \$3,500.0 \$3,500.0 \$3,500.0 \$21,000.0 \$10,250.1 \$120,000.1 \$10,250.1 \$10,200.1 \$10,200.1 \$10,200.1 \$10,200.1 \$10,200.1 \$10,200.1
607-3001 608-3000 509-3120 607-3001 608-3000 608-3000 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 608-3000 509-3120 509-3120 509-3120 509-3120 509-3120 509-3120	Wall #9 Conc. Gravity Walls GDOT 90311.	\$1a. 94+37 to sta. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 212" x 24" cap granite rubble, ashlar pattern sta. 88-34 to sta. 87-40 (94 If, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-78 to sta. 88-75 (103 If, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86-86 to sta. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 If, 2-3' avg. ht x 8" wide) sta. 65+59 to 62+47 (312 If, 6' avg hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 62+47 to 61+85 (62 If, 6' avg. hgt.x 8" wide) sta. 65+59 to 59-570 (215 If, 7.5' avg. hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 63+85 to 59-32 (88 If x 2' avg. hgt.x 8' wide) sta. 59-37 to 55+32 (88 If x 2' avg. hgt.x 8' wide) sta. 59-37 to 55+32 (88 If x 2' avg. hgt.x 12" wide)		150 265 1300 60 100 550 70 100 600 77 80 312 1872 10 68 8 110 68 115 163 3 3 121	UF SF CY CY CY CY CY CY CY UF SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00 \$35.00	\$9,275.0 \$45,000.0 \$34,000.0 \$3,500.0 \$19,250.0 \$35,500.0 \$35,500.0 \$10,250.0 \$10,250.0 \$10,250.0 \$15,500.
607-3001 608-3000 500-3120 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 608-3000 500-3120 608-3000 500-3120 608-3000	Wall #9 Conc. Gravity Walls GDOT 90311.	\$1a. 944-37 to \$1a. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 88-34 to \$1a. 87-40 (94 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86-78 to \$1a. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86-78 to \$1a. 88-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 86-78 to \$2-47 (312 If, 6' avg hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 62-47 to \$18-85 (62 If, 6' avg hgt.x 2" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1a. 59-70 to \$16-32 (38 If x 2' avg hgt.x 2" wide) \$1a. 59-70 to \$59-32 (38 If x 2' avg hgt.x 8" wide) \$1a. 59-70 to \$59-32 (38 If x 2' avg hgt.x 8" wide) \$1a. 59-70 to \$59-32 (38 If x 2' avg hgt.x 8" wide) \$1a. 59-70 to \$59-32 (38 If x 2' avg hgt.x 8" wide) \$1a. 59-70 to \$59-32 (38 If x 2' avg hgt.x 8" wide)		150 265 1300 60 1000 550 70 1000 600 7 80 812 1872 10 688 215 1613 3 3 121 362 2900	LF SF CY CY CY LF SF CY CY LF SF SF CY CY LF SF SF CY CY CY LF SF SF CY CY CY CY LF SF	\$35.00 \$35.00 \$34.00.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$19,250.0 \$35,000.0 \$21,000.0 \$21,000.0 \$10,850.0 \$10,850.0 \$10,250.
607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000 500-3120 607-3001 608-3000	Wall #9 Conc. Gravity Walls GDOT 90311.	sta. 94+37 to sta. 91+71 (266 lf, 5' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 88+34 to sta. 87+40 (94 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+76 to sta. 85+75 (103 lf, 5-6'avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 86+75 to sta. 85+75 (103 lf, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granite rubble, ashlar pattern sta. 84+62 to 83+75 (87 lf, 2-3' avg. ht x 8" wide) sta. 65+59 to 62+47 (312 lf, 6' avg hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 62+47 to 61+85 (62 lf, 6' avg hgt.x 8" wide) sta. 63+85 to 59+70 (215 lf, 7.5' avg. hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 59+97 to 59+32 (83 lf, x 2' avg. hgt.x 8" wide) sta. 59+32 to 55+70 (362 lf, 8' avg. hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 59+72 to 52+70 (362 lf, 8' avg. hgt.x 12" wide) 12" x 24" cap granite rubble, ashlar pattern sta. 54+25 to 52+32 (102 lf, 6' avg. hgt.x 12" wide)		150 2655 1300 60 100 550 100 600 7 7 80 312 1872 100 68 215 1613 3 3 121 111 362 2900 277	LE SF CY	\$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.0 \$45,000.0 \$34,000.0 \$3,500.0 \$19,220.0 \$35,000.0 \$21,000.0 \$10,280.0 \$110,200.0 \$10,200.0 \$110,200.0 \$15,00
607-3001 608-3000 500-3120	Wall #9 Conc. Gravity Walls GDOT 90311. Wall #9 Stone Veneer Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #10 Stone Cap Wall #10 Stone Veneer Wall #11 Stone Cap Wall #18 Stone Cap Wall #18 B Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Cap Wall #18 Stone Cap Wall #18 Stone Cap Wall #38 Stone Cap Wall #38 Stone Cap Wall #38 Stone Cap Wall #38 Stone Veneer Wall #38 Stone Venee	\$1a. 944-37 to \$1a. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 884-34 to \$1a. 837-40 (94 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 864-78 to \$1a. 854-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 864-78 to \$1a. 854-75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 864-78 to 834-75 (87 If, 2-3' avg. ht x 8" wide) \$1a. 864-80 to 834-75 (87 If, 6' avg hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 654-95 to 61-85 (62 If, 6' avg hgt.x 8" wide) \$1a. 654-95 to 61-85 (62 If, 6' avg hgt.x 8" wide) \$1a. 654-95 to 61-85 (62 If, 6' avg hgt.x 8" wide) \$1a. 654-95 (10 362 If, 8' avg hgt.x 8" wide) \$1a. 559-70 (253 If, 8' avg hgt.x 8" wide) \$1a. 559-70 (253 If, 8' avg hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 549-70 to 554-70 (125 If, 8' avg hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1a. 542-55 to 534-23 (102 If, 6' avg hgt.x 12" wide)		150 265 1300 60 1000 550 100 070 070 80 812 1872 106 68 215 1613 3 3 121 121 2900 27	正	\$35.00 \$35.00 \$1,400.00 \$35.00	\$9,275.0 \$45,500.0 \$84,000.0 \$3,500.0 \$3,500.0 \$19,250.0 \$3,500.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0 \$11,000.0
607-3001 608-3000 508-3120 607-3001 608-3000 508-3120 607-3001 608-3000 508-3120 607-3001 608-3000 508-3120 607-3001 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3120 608-3000 508-3000 508-3000 508-3000 508-3000 508-3000 508-3000 508-3000 508-3000 508-3000	Wall #9 Conc. Gravity Walls GDOT 90311. Wall #9 Stone Cap Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #11 Stone Cap Wall #10 Stone Veneer Wall #11 Stone Cap Wall #11 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #11 Stone Cap Wall #11 Stone Veneer Wall #12 Turndown Concrete Slab with Reinforcement w/cable handrail Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Veneer Wall #18 Poured in Place Wall w/picket fence Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Veneer	\$13. 944-37 to \$13. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$13. 88+34 to \$13. 87+40 (98 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 86+78 to \$13. 85+75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+62 to \$1. 83+75 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1. 84+62 to \$1. 83+75 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1. 86-24*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*5 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$13. 85*70 to \$1. 85*70 (362 If, 8' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 86+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern		150 150	日	\$35.00 \$35.00 \$31,400.00 \$35.00	\$9,275.0 \$45,000 \$34,000.0 \$35,000 \$35,000 \$35,000 \$21,000.0 \$10,850.0 \$10,850.0 \$10,850.0 \$10,850.0 \$10,900.0 \$10,9
907-3001 908-3000 908-3000 908-3000 908-3120 907-3001 908-3000 908-3120 908	Wall #9 Conc. Gravity Walls GDOT 90311.	\$1.3.94+37 to \$1.3.91+71 (266 If, 5' avg. hgt.x 2.5' wide) granter rubble, ashlar pattern \$1.3.88-34 to \$1.3.87+40 (98 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granter rubble, ashlar pattern \$1.867+81 to \$1.85.87+5 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap granter rubble, ashlar pattern \$1.867+81 to \$847-5 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap granter rubble, ashlar pattern \$1.864+62 to \$847-5 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap granter rubble, ashlar pattern \$1.862+47 to \$1.85 (52 If, 6' avg hgt.x 12" wide) 12" x 24" cap granter rubble, ashlar pattern \$1.862+47 to \$1.85 (52 If, 6' avg hgt.x 8" wide) \$1.864-82 to \$947-82 (28 If x 2' avg. hgt.x 12" wide) 12" x 24" cap granter rubble, ashlar pattern \$1.863-97 to \$559-70 (235 If, 8' avg. hgt.x 8" wide) \$1.863-97 to \$559-70 (325 If, 8' avg. hgt.x 12" wide) 12" x 24" cap granter rubble, ashlar pattern \$1.863-97 to \$559-70 (325 If, 8' avg. hgt.x 12" wide) 12" x 24" cap granter rubble, ashlar pattern \$1.864-97 to \$1.97 to \$		150 2655 1300 60 1000 550 70 100 600 77 80 312 1872 2155 1613 33 121 121 362 2900 2900 612	LE SF CY CY CY CY CY CY LE SF CY	\$35.00 \$35.00 \$31,400.00 \$35.00	\$9,275.6 \$45,000.1 \$84,000.0 \$84,000.0 \$98,000.0 \$98,000.0 \$19,250.0 \$10,250.0 \$10,250.0 \$10,250.0 \$15,000.0 \$7,525.6 \$4,950.0 \$110,000.0 \$110,
107-3001 108-3000 108-3000 108-3000 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 107-3001 108-3000 109-3120 109	Wall #9 Conc. Gravity Walls GDOT 90311. Wall #9 Stone Cap Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #10 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #11 Stone Cap Wall #10 Stone Veneer Wall #11 Stone Cap Wall #11 Conc. Gravity Walls GDOT 90311. w/picket fence Wall #11 Stone Cap Wall #11 Stone Veneer Wall #12 Turndown Concrete Slab with Reinforcement w/cable handrail Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Veneer Wall #18 Poured in Place Wall w/picket fence Wall #18 Poured in Place Wall w/picket fence Wall #18 Stone Cap Wall #18 Stone Veneer	\$13. 944-37 to \$13. 91+71 (266 If, 5' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$13. 88+34 to \$13. 87+40 (98 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 86+78 to \$13. 85+75 (103 If, 5-6' avg. hgt.x 2.5' wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+62 to \$1. 83+75 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1. 84+62 to \$1. 83+75 (87 If, 2-3' avg. ht x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$1. 86-24*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*5 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$13. 86+25*7 to \$1. 85*5 (62 If, 6' avg. hgt.x 8" wide) \$12" x 24" cap graniter rubble, ashlar pattern \$13. 85*70 to \$1. 85*70 (362 If, 8' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 86+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern \$1. 84+25*5 to \$1. 83+23 (102 If, 6' avg. hgt.x 12" wide) 12" x 24" cap graniter rubble, ashlar pattern		150 150	日	\$35.00 \$35.00	\$9.275.6 \$45,000.6 \$3,500.0 \$3,500.0 \$98,000.0 \$21,000.0 \$10,250.0

138 ROBERTS DRIVE SIDE PATH SCOPING REPORT **CITY OF SANDY SPRINGS** 139



PHASE 2 PREFERRED, SHEET 2 OF 2

CONCEPTUAL COST ESTIMATE

(DBERTS DRIVE - Phase 2 - PREFERRED

toswell Road to end of Phase 1

tation 118+95 to Station 53+10 = (6,585 LF = 1.2 miles

ITEM Curb Cut Wheelchair Ramp, Type A Curb Cut Wheelchair Ramp, Type B			NUMBER 3	UNIT EA EA	COST/UNIT \$2,800.00 \$3,700.00	TOTAL \$8,400
Curb Cut Wheelchair Ramp, Type C		+	0	EA	\$2,400.00	\$0
Eurb Cut Wheelchair Ramp, Type D DETECTABLE WARNING SURFACE	warning surface 8 SF/ea (24"x48" typ.)		18 144	EA SF	\$2,000.00 \$35.00	\$36,000 \$5,040
Boardwalk with railing	Two boardwalks sections #1 sta 108+18 (148 LF) and #2 sta. 87+40 (62 LF	,	210	LF	\$2,250.00	\$472,500
iignage on side path					Subtotal	<u>\$521,940</u>
Bicycles only Wayfinding Signage		#	0	EA	\$1,000.00 \$1,000.00	\$4,000
Mile Markers Yellow Centerline			0		\$1,000.00 \$3.00	\$0
Roadway Concrete Driveway Apron (8" Thick)	14 driveways 20' x 25' typ. =500 sf typ. X 14 + 7000 sf/9 = 778 sy		778	SY	Subtotal \$125.00	\$4,000 \$97,250
		Ħ				
Concrete Curb and Gutter 8" x 30" Tp 2 Recycled Asph Conc 9.5 MM Superpave, Type II, GP 2 Only, Incl Bituminous Mati & H Lime(2")	10 radius at 35' LF each typ. + road diet (sta 58+55 to 56+40) 22 ft wide x 315 H* = 6930 slf 770 syl/sta. 58+55 - 56+40) (*includes 100 lf of transition)		665	LF TN	\$30.00 \$205.00	\$19,950 \$16,400
Recycled Asph Conc 12.5 MM Superpave, GP 2 Only, Incl Matl & H Lime (1.5") MILL ASPH CONC PVMT, VARIABLE DEPTH	22 ft wide x 315 if = 6930 sf(770sy)		60	TN SY	\$200.00 \$20.00	\$12,000 \$0
igning and Marking	·				, , , , , , , , , , , , , , , , , , ,	·
Stop Sign	locations at drive exits		8	EA	\$1,200.00	\$9,60
Multiple traffic signs Stop Bar	roadway 8 x 12 lf (drives)		10 96	EA LF	\$1,200.00 \$6.00	\$12,00 \$57
Yellow Centerline Rectangular Rapid Flashing Beacon	sht 5B road diet (sta. 58+55 - 56+40) = 215 lf + transition Mid Block Crossing	Ħ	315	LF EA	\$3.00 \$3.00 \$25,000.00	\$94
Concrete Median 6" Traffic Control Signage	Phase 1 only midblock crossing	-	0	EST	\$20,000.00 \$500.00	\$ \$1,00
Utilities					Subtotal	\$194,721
Relocate Fire Hydrant Proposed Storm Manhole			4	EA EA	\$3,500.00 \$2,500.00	\$14,00 \$
Drop Inlet GP1 GD0T 1019A Storm Pipe 18" RCP			0	EA LF	\$3,500.00 \$75.00	\$
Storm Pipe 24" RCP Concrete End Section		++	0	LF EA	\$100.00 \$5,000.00	\$
Rip Rap			0	TN	\$85.00 Subtotal	\$ \$14,00 0
Site Amenities Furnishings			1			
Bollard Bench	1 at each intersection corner at trail entries 2 at intersection with Roswell Road	+	19	EA EA	\$1,500.00 \$4,000.00	\$28,50 \$8,00
Trash Receptacle	1 at intersection with Roswell Road	\mp	1	EA	\$2,500.00	\$2,50
Cable Handrailing (turndown slabs only) Picket Fencing (walls > 6' ht)	42" Cable Handrailing 48" ht top of walls >6' ht	+	160 1863	LF LF	\$200.00 \$125.00	\$32,00 \$232,87
andscape	<u> </u>		•		Subtotal	\$303,87
Plant Topsoil	4" for sod areas + plants	Ш	185	CY	\$50.00	\$9,25
Landscape Mulch	4" depth	Ш	440	SY	\$20.00	\$8,80
Sod	bermuda		868	SY	\$15.00	\$13,02
THE PARTY OF						
Street Tree	45 gal., 3" cal., variety TBD	++	36	EA	\$1,500.00	\$54,000
shrubs (3' o.c.)	3 gal, variety TBD		963	EA	\$40.00	\$38,52
					Subtotal	\$123,590
		П	-		Subtotal Project Subtotal	
		O&P/	Proj Mgmt/M	obilization/Bo		\$5,989,073 \$1,497,26 \$1,197,81
		O&P/	Proj Mgmt/M		Project Subtotal inding & Insurance (25%)	\$5,989,073 \$1,497,26 \$1,197,81 \$718,68
tems Gulside Construction Contract		O&P/	Proj Mgmt/M		Project Subtotal ending & Insurance (25%) 20 % Contingency ginieering Services (12%)	\$5,989,073 \$1,497,26 \$1,197,81 \$718,68
Utility Pole Relocations and Pedestrian Lighting	To reliev used	O&P/		Design & En	Project Subtotal anding & Insurance (25%) 20 % Contingency ginieering Services (12%) Total	\$5,989,07: \$1,497,26 \$1,197,81 \$718,68 \$9,402,845
Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole	12 relocated 1 relocate light	O&P/	Proj Mgmt/M		Project Subtotal ending & Insurance (25%) 20 % Contingency ginieering Services (12%)	\$5,989,07: \$1,497,26 \$1,197,81 \$718,68 \$9,402,845
Utility Pole Relocations and Pedestrian Lighting Relocate Utility Pole Relocate light pole, add utility pole	1 relocate light	O&P/	10	Design & En	Project Subtotal Inding & Insurance (25%) 20 % Contingency ginleering Services (12%) Total \$25,000.00 \$10,000.00	\$5,989,073 \$1,497,26 \$1,197,81 \$718,68 \$9,402,845 \$250,00 \$10,00
Jülliy Põe Relocations and Pedestrian Lighting Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole		O&P/	10 1 76	Design & En	Project Subtotal Inding & Insurance (25%) 2.0 % Contingency ginieering Services (12%) Total \$25,000.00 \$10,000.00 \$12,000.00 \$12,000.00	\$5,989,073 \$1,497,26 \$1,197,81 \$718,68 \$9,402,845 \$250,00 \$10,00 \$912,00 \$515,00
JUBIN POR REOCATIONS and Pedestrian Lighting Relocate Utility Pole Relocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Conduit NONMETA, T.P. P., 2 IN and Elect. Wire	1 relocate light 13' DECORATIVE	O&P/	100 1 766 1 1 100 6500	EA EA LF	Project Subtotal anding & Insurance (25%) 20% Continuering Services (12%) 70% Continuering Services (12%) Total \$22,000.000 \$10,000.000 \$15,000.000 \$515,000.000 \$20.000 \$515,000.000 \$20.000 \$515,000.000 \$20.000 \$515,000.000 \$20.00	\$5,989,072 \$1,497,26 \$1,197,81 \$718,88 \$9,402,845 \$250,00 \$10,00 \$10,00 \$11,00 \$15,00 \$2,00 \$15,00 \$15,00
Utility Pole Redocations and Redestrian Lighting Rebocate Utility Pole Redocate Utility Pole Add Pedestrian Light Pole Directional Bore Conduit NONMETT, TP 2, 2 IN and Elect. Wire Electrical Service Point Lectrical Service	1 reboate light 13' DECORATIVE 18' DECORATIVE GATEWAY	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En	Project Subtotal Minusmone (25%) 20% Configured (25	\$5,989,072 \$1,497,26 \$1,197,81 \$718,68 \$9,402,845 \$250,00 \$10,00 \$11,00 \$11,00 \$312,00 \$37,50 \$37,50 \$52,00
Utility Pole Redocations and Redestrian Lighting Rebocate Utility Pole Redocate Utility Pole Redocate light pole, add utility pole Add Pedestrian Light Pole Directional Bore Condult NONMETT, TP 2, 2 IN and Elect. Wire Electrical Service Point Caddle, TP TPA AND NO TBD Redocate Electrical Box	1 relocate light 13' DECORATIVE	0&P/	10 1 76 1 100 6500 2	Design & En	Project Subtotal ading & Invariance (25%) 20% Confliction 20% Confliction 20% Confliction 325,000.00 510,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00	\$5,989,072 \$1,497,26 \$1,197,81 \$718,68 \$9,402,845 \$250,00 \$10,00 \$11,00 \$15,00 \$250,00 \$37,50 \$37,50 \$57,50 \$50,00 \$50,00 \$50,00 \$50,00
Julian Pole Relocations and Pedestrian Lighting Relocate Utility Pole Directional Bore Londin NonMarth, T.P. 2, 2 N and Elect. Wire Relocate Service Point Relocate Utility Relocate Utility Relocate Relocate Electrical Box Rights Of-Way and Exement Cost Rights Of-Way and Exement Cost	1 reboate light 13' DECORATIVE 18' DECORATIVE GATEWAY	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EA EA EA LF EA LF EA LF EA	Project Subtotal anding & Insurance (25%) 28 Kontinger (25%) 28 Kontinger (25%) 27 Kontin	\$5,989,072 \$1,487,26 \$1,197,26 \$718,68,65 \$3,402,845 \$250,00 \$10,00 \$11,00 \$11,00 \$2,00 \$2,00 \$3,20 \$3
JUBIN POR Relocations and Pedestrian Lighting Relocate Uility Pole Relocate Uility Pole Relocate Light pole, and utility pole Add Pedestrian Light Pole Directional Relocation Light Pole Directional Relocation Light Relocation Light Relocation	1 relocate light 13' DECORATIVE 13" DECORATIVE GATEWAY To be determined	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EA EA EA LF EA LF EA LF EA	Project Subtotal inding & Insurance (25%) 20% Confliction 20% Confliction 30% Confliction 525,000.00 510,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00 515,000.00	\$5,989,072 \$1,487,26 \$1,197,26 \$718,68,65 \$3,402,845 \$250,00 \$10,00 \$11,00 \$11,00 \$2,00 \$2,00 \$3,20 \$3
Julian Pole Relocations and Pedestrian Lighting Relocate Utility Pole Directional Bore Londin NonMarth, T.P. 2, 2 Na and Elect. Wire Relocate Electrical Box Relocate Electrical Box Relocate Electrical Box Rights Of-Ways and Essement Cost Rights Of-Ways and Essement Cost Rights Of-Ways and Essement Cost	1 reboate light 13' DECORATIVE 18' DECORATIVE GATEWAY	O&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EA EA EA LF EA LF EA LF EA	Project Subtotal anding & Insurance (25%) 28 Kontinger (25%) 28 Kontinger (25%) 27 Kontin	\$5,989,072 \$1,497,26 \$1,197,26 \$718,68 \$9,402,84\$ \$1,200 \$110,
JUBIN POR Relocations and Pedestrian Lighting Relocate Utility Pole Relocate Utility Pole Relocate Utility Pole Relocate Light pole, add utility pole Relocate Light Pole Development L	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DROSCOSCITICATION, DROSCOSCIT	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En	Project Subtotal Project Subtotal anding & insurance (25%); 20% Confined Subtotal 20% Confined Subtotal Subtota	\$5,989,073 \$1,497,26 \$1,497,26 \$1,497,26 \$718,68 \$59,402,845 \$250,000 \$10,000 \$11,000 \$11,000 \$512,000 \$650,000 \$650,000 \$650,000 \$650,000 \$650,000 \$51,371,500
Utility Pole Relocate	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined New, reasonablishings, results are consistent of the construction of t	0&P/	10 1 766 1 1 100 0 5500 0 0	EA E	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 25% 25% 25% 25% 25% 25% 25% 25% 25% 25%	\$5,989,07: \$1,497,26 \$1,197,26 \$718,68 \$9,402,84\$ \$250,000 \$10,000 \$11,000 \$512,000 \$513,000
Single Family Residential Multi-Family Residential - Apartments	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DROSCOSCITICATION, DROSCOSCIT	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EA EA EA LF EA LF EA EA SF EA SF SF SF	Project Subtotal Minariane (25%); 20% Configure (25	\$5,989,07: \$1,497,26 \$1,197,26 \$718,68 \$9,402,84\$ \$250,000 \$10,000 \$11,000 \$512,000 \$513,000
Utility Pole Relocate Relocate Utility Pole Relocate Relocate Utility Relocate Relocate Relocate Relocate Relocate Relocate Relocate Boox Relocate Relocate Boox Relocate Relo	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DROSCOSCITICATION, DROSCOSCIT	08.97	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EAA EAA EAA SF	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configure (25%) 20% Con	\$5,989,07; \$1,497,26; \$1,497,26; \$1,497,26; \$1,497,26; \$1,497,26; \$1,200,000 \$10,000 \$10,000 \$11,000 \$
JUBIN POR Relocations and Pedestrian Lighting Relocate Ulitiry Pole Relocate Ulitiry Relocate Relocate Ulitiry Relocate Relocate Relocate Relocate Relocate Relocate Boox Relocate Relocate Boox Relocate Relocate Boox Relocate Relocate Boox Relocate Reloc	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DROSCOSCITICATION, DROSCOSCIT	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En EA EA EA EA FA FA FA FA FA FA	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 25%	\$5,989,07: \$1,497,26 \$1,1497,26 \$1,1497,26 \$1,1497,26 \$1,1497,26 \$1,200,00 \$
Julian Pole Relocations and Pedestrian Lighting lebecate Utility Pole lebecate Utility Pole lebecate Utility Pole lebecate light pole, and the lebecate lebecat	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DRIEGOSCITICATION, DRIEGOSCIT	0&P/	100 11 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En EA EA EA EA EA EA EA SF SF SF SF SF SF SF SF	Project Subtotal Project Subtotal anding & insurance (25%); 20% Configured (25%); 20% Co	\$5,989,073 \$1,497,26 \$1,497,26 \$718,08 \$718,08 \$59,402,845 \$250,000 \$10,000 \$11,000 \$1
JUBIN POR Relocations and Pedestrian Lighting Relocate Ulitiry Pole Relocate Ulitiry Relocate Relocate Ulitiry Relocate Relocate Relocate Relocate Relocate Relocate Boox Relocate Relocate Boox Relocate Relocate Boox Relocate Relocate Boox Relocate Reloc	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DRIEGOSCITICATION, DRIEGOSCIT	O&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En EA EA EA EA FA FA FA FA FA FA	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 25%	\$5,989,07: \$1,497,26 \$1,1497,26 \$1,1497,26 \$1,1497,26 \$1,1497,26 \$1,200,00 \$
Julian Pole Relocations and Pedestrian Lighting lebecate Utility Pole lebecate Utility Pole lebecate Utility Pole lebecate light pole, and the lebecate lebecat	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined W. GROSCOSCITICATION, DRIEGOSCITICATION, DRIEGOSCIT	O&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Design & En EA EA EA EA FA FA FA FA FA FA	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 25%	\$5,989,073 \$1,497,26 \$1,497,26 \$718,088 \$718,088 \$59,402,845 \$250,000 \$10,000 \$11,000
Julian Pole Relocations and Pedestrian Lighting belocate Utility Pole belocate Utility Pole belocate light pole, and utility pole belocate light pole belocate li	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined To be determined PAY: 0000000000117004, 0001500000014409, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 0001500514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EAA	Project Subtotal Project Subtotal anding & Insurance (25%), 20% Configured (25%), 20% Co	\$5,989,07: \$1,497,265 \$1,497,265 \$1,197,265 \$5,402,845 \$2,500,000 \$11,
Juliany Fold Redocations and Pedestitan Lighting Lebotate Utility Pole Lebotate Utility Pole Lebotate Light pole, and utility pole Lebotate Light pole, and utility pole Lebotate Light Pole Lebo	11 relocate light 13 DECORATIVE 18 DECORATIVE GATEWAY To be determined PAY: 1000/2009/11/2004, 2000/2000/2014/004, 2000/2000/2014/01/2014/	08.9/	100 100 100 100 100 100 100 100 100 100	EA E	Project Subtotal Project Subtotal anding & Insurance (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 20% Configured (25%) 25% Configure	\$5,989,07: \$1,497,265 \$1,497,265 \$1,197,265 \$5,402,845 \$2,500,000 \$11,
Julian Pole Relocations and Pedestrian Lighting belocate Utility Pole belocate Utility Pole belocate light pole, and utility pole belocate light pole belocate li	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined To be determined PAY: 0000000000117004, 0001500000014409, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 0001500514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000	08.P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EAA	Project Subtotal Project Subtotal anding & Insurance (25%), 20% Configured (25%), 20% Co	\$5,989,07: \$1,497,265 \$1,497,265 \$1,197,265 \$5,402,845 \$2,500,000 \$11,
Julian Pole Relocations and Pedestrian Lighting belocate Utility Pole belocate Utility Pole belocate Light pole, and utility pole belocate Light pole, and utility pole belocate Light pole, and utility pole belocate Light Pole Directional Bore of Conduct NONMETL, TP 2, 2 IN and Elect. Wire lectrical Sorvice Point Light Pole Pole Pole Conduct NONMETL, TP 2, 2 IN and Elect. Wire lectrical Box Direction Light Pole Pole Pole Pole Pole Pole Pole Pole	1 relocate light 13' DECORATIVE 18' DECORATIVE GATEWAY To be determined To be determined PAY: 0000000000117004, 0001500000014409, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 00015000000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 0001500000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 000150000514239, 0001500514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000514239, 00015000	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EAA	Project Subtotal anding & Insurance (25%) 28 Configure (25%) 28 Configure (25%) 29 Config	\$5,989,07: \$1,872,62 \$1,1972,62 \$718,68 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$3,100,000 \$3,1
Julia y Fole Relocations and Pedestitan Lighting lecionate Utility Pole elecionate Utility Pole elecionate light pole, and utility pole elecionate light pole electron	11 relocate light 11 DECORATIVE 18 DECORATIVE GATEWAY To be determined Nav. +00000000011(700-), 000010000014444, 000010000015(170-), 0000100000114444, 000010000015(170-), 00001000000112713-1, 00001000000012713-1, 0000100000012713-1, 0000100000012713-1, 00001000000012713-1, 000010000000000000000000000000000000	0&P/	100 100 100 100 100 100 100 100 100 100	EA E	Project Subtotal anding & Insurance (25%) 20 % Configure (25%) 20 % Conf	\$5,989,07 \$1,407,26 \$1,107,26 \$1,107,26 \$1,107,26 \$1,100,00 \$11,0
Julian Pole Relocations and Pedestrian Lighting belocate Utility Pole belocate Utility Pole belocate Light pole, and utility pole belocate Light pole, and utility pole belocate Light pole, and utility pole belocate Light Pole Directional Bore of Conduct NONMETL, TP 2, 2 IN and Elect. Wire lectrical Sorvice Point Light Pole Pole Pole Conduct NONMETL, TP 2, 2 IN and Elect. Wire lectrical Box Direction Light Pole Pole Pole Pole Pole Pole Pole Pole	11* DECORATIVE 12* DECORATIVE GATEWAY 13* DECORATIVE GATEWAY To be determined To be determined	0&P/	100 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EAA	Project Subtotal anding & Insurance (25%) 28 Configure (25%) 28 Configure (25%) 29 Config	\$5,989,07; \$1,497,26; \$1,497,26; \$1,197,26; \$1,197,26; \$1,197,26; \$1,197,26; \$1,200,000 \$10,000 \$11,00
JUSTIA POR RECOLOTORS and Pedestrian Lighting Recicate Utility Pole Relocate Utility Relocate Relocate Utility Relocate Relocate Relocate Boox Relocate Re	11 relocate light 11 DECORATIVE 18 DECORATIVE GATEWAY To be determined Nav. +00000000011(700-), 000010000014444, 000010000015(170-), 0000100000114444, 000010000015(170-), 00001000000112713-1, 00001000000012713-1, 0000100000012713-1, 0000100000012713-1, 00001000000012713-1, 000010000000000000000000000000000000	0.8.P/	100 100 100 100 100 100 100 100 100 100	EA E	Project Subtotal anding & Insurance (25%) 20 % Configure (25%) 20 % Conf	\$5,989,072 \$1,497.26 \$1,197.26 \$718,68 \$29,402,845 \$29,402,845 \$29,402,845 \$31,200,000 \$10,000 \$10,000 \$10,000 \$11,000
Julia y Bole Relocate Unity Pole Relocate Utility Relocate Re	13* DECORATIVE 13* DECORATIVE 13* DECORATIVE GATEWAY To be determined Nav. neon-inconsort: (Nov.). neon-inconsort-senio, neon-inconsort-strain, neon-inconsort: (Nov.). neon-inconsort-strain, neon-incons	0.8.P/	10 10 10 11 11 11 11 11 11 11 11 11 11 1	EA E	Project Subtotal anding & Insurance (25%) 20% Configure 20% Configure 525,000.00 \$10,000.00 \$10,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000.00 \$11,000 \$	\$5,989,073 \$1,497,26 \$1,497,26 \$1,497,26 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$2,402,845 \$3,402,845 \$4,
JUBIN Pole Relocate Utility Pole Relocate Utility Pole Relocate Light pole, and utility pole Relocate Light pole, and utility pole Relocate Light pole, and utility pole Relocate Light pole Relocate Light Pole Description D	11 relocate light 11 DECORATIVE 18 DECORATIVE GATEWAY To be determined Nav. +00000000011(700-), 000010000014444, 000010000015(170-), 0000100000114444, 000010000015(170-), 00001000000112713-1, 00001000000012713-1, 0000100000012713-1, 0000100000012713-1, 00001000000012713-1, 000010000000000000000000000000000000	08.97	10 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	EA	Project Subtotal anding & insurance (25%) 20 % Contings of the Continuation of the Con	\$2,000 \$97,500 \$20,000 \$65,000 \$1,371,500 \$1,371,500 \$167,620 \$494,500 \$1,500 \$

07.5 PHASE 2 ALTERNATIVES COMPARISON

PHASE 2: PREFERRED vs ALTERNATIVE STA 53+10 TO 78+00

PREFERRED

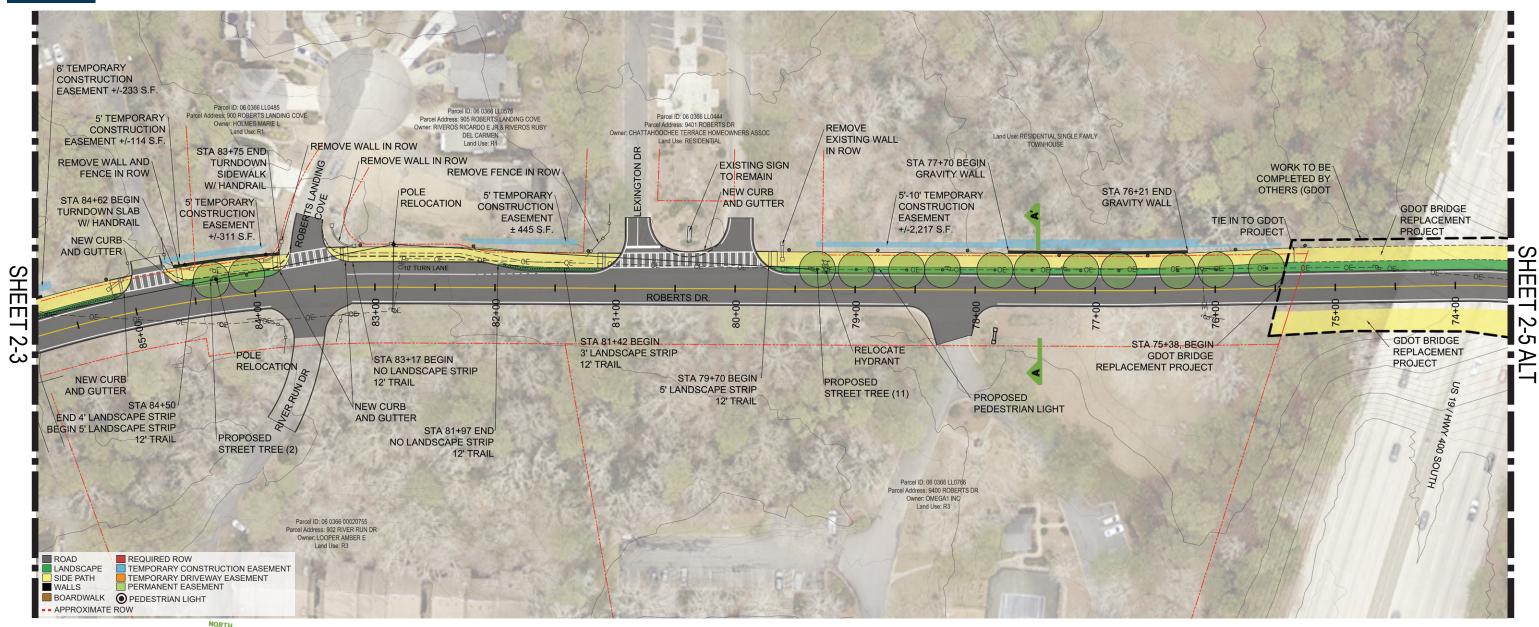
- Path is adjacent to multi-family apartment complex, single residential housing, and wooded areas
- Path will be located on the side of Roberts Drive with neighborhoods, offering access for users who live along this segment of the path
- Users will cross the Roberts Drive two additional times when traversing the path
- 1 additional mid-block crossing (4 total mid-block crossings within entire project)
- This section of path impacts 4 parcels
- Additional right-of-way and temporary construction easements are required
- Path will cross 3 drives, 1 residential property and two apartment complex entrances
- Existing sidewalk will need to be removed
- There are 7 walls in this section of path: 4 poured-in-place walls (312' +/-, 215' +/-, 362' +/-, and 102' +/-), and 3 turn down curbs (62' +/-, 38' +/-, and 108' +/-)
- There are 3 power pole and 2 fire hydrant relocations in this section of path
- There will be approximately 566 shrubs and 17 trees planted along this route

ALTERNATIVE

- Path is adjacent to National Park property, offering views of forest and nature
- On this section of the path, users from residential areas on the west side, would be required to cross Roberts Drive to access the side path on the east side at the one midblock crossing.
- No additional mid-block crossings (3 total mid-block crossings within entire project)
- This section of path impacts 3 parcels (2 are owned by the National Park Service)
- Only temporary construction easements are required for this section
- Path does not cross any drives
- No existing sidewalk to remove
- There are 4 walls in this section of path: 1 gravity wall (150' +/-), 1 turn up curbs (285' +/-), and 2
- turn down curbs (101' +/- and 100' +/-)
- There are 5 power pole relocations in this section of path
- There will be approximately 794 shrubs and 10 trees planted along this route

140 ROBERTS DRIVE SIDE PATH SCOPING REPORT CITY OF SANDY SPRINGS 141

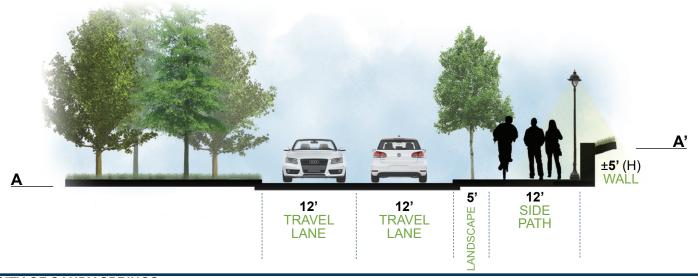
07.6 PHASE 2 ALTERNATIVE DESIGN (STA 85+50 TO 75+38)



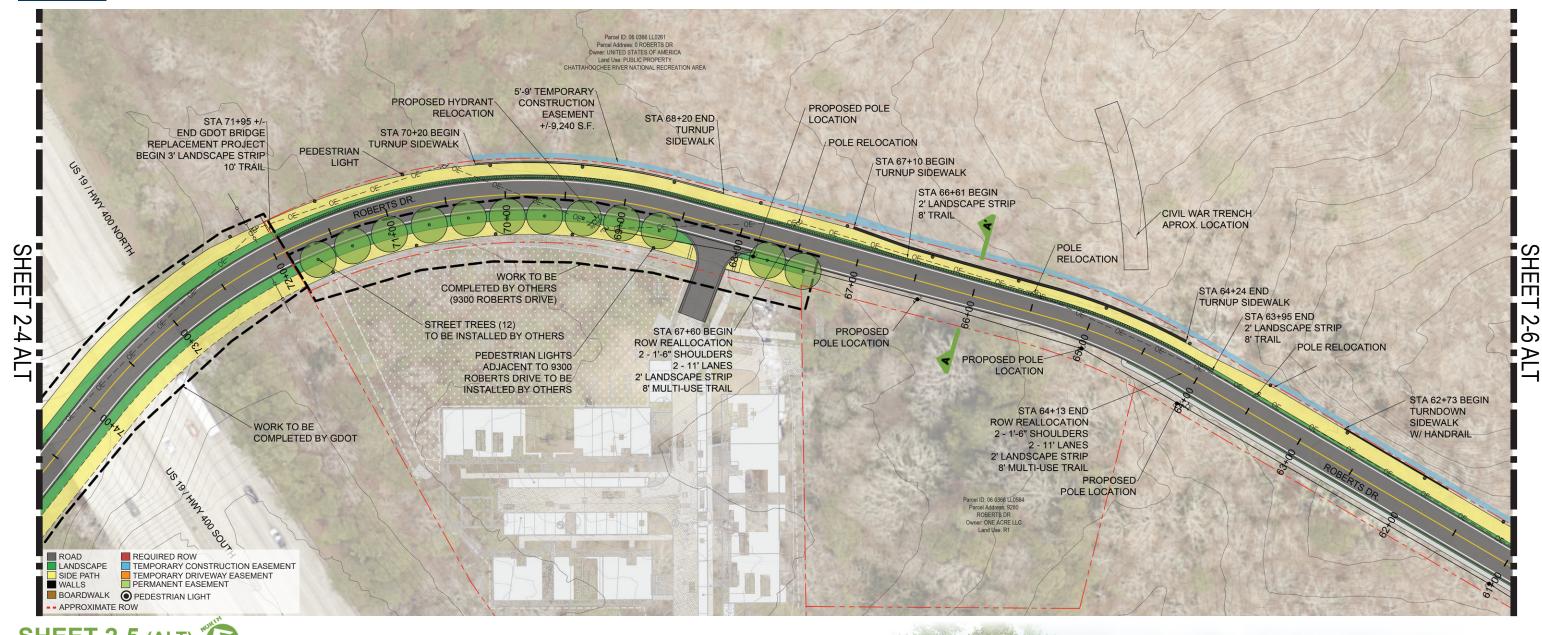
SHEET 2-4 (ALT)



This sheet shows path alignment for Phase 2 Alternative, where the side path remains continuous on the north side of Roberts Drive. This segment continues at STA 85+50 and ends at STA 75+38. The path continues at 12 ft. wide with a 5 ft. landscape strip, with street trees. As the path travels along the deceleration lane, the landscape strip is removed to minimize required space. A 3 ft. landscape strip begins at STA 81+97 and continues to STA 81+42. After crossing Lexington Drive, the path continues at 12 ft. wide with a 5 ft. landscape strip, with street trees (section A-A'). This sheet ends at the start to the GDOT bridge replacement project, which will provide a 12 ft. wide side path with a 8' landscape strip on both sides of Roberts Drive. Topography changes will require a turn-down slab and a gravity wall. Temporary construction easements are required within this segment, as well as utility relocations.



07.6 PHASE 2 ALTERNATIVE DESIGN (STA 71+95 TO 61+00)

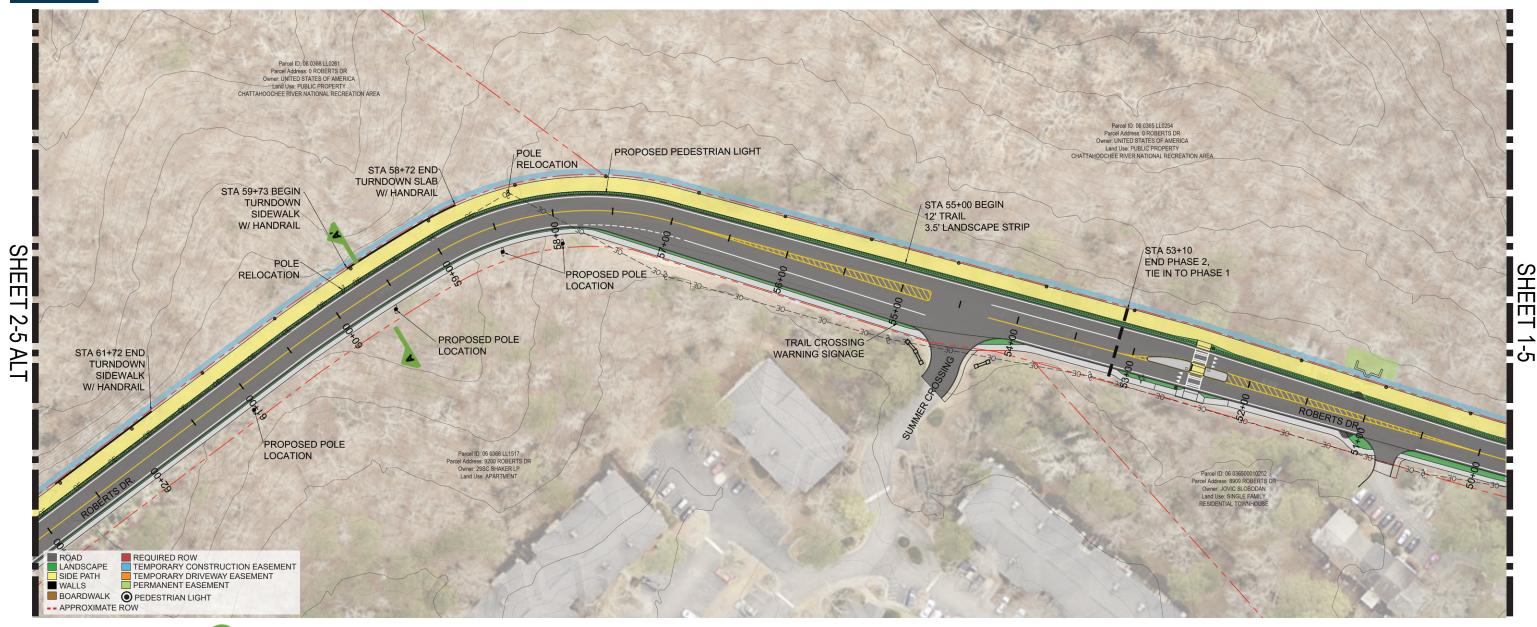


SHEET 2-5 (ALT)

This sheet shows path alignment Phase 2 Alternative, where the side path remains continuous on the north side of Roberts Drive. This segment continues at STA 71+95, where the GDOT bridge replacement project ends, and continues to STA 61+00. The path begins at 10 ft. wide with a 3 ft. greenspace. To avoid sensitive areas within the CRNRA property, a reallocation of right of way is proposed to gain extra space for the path between STA 68+00 and STA 63+50. The narrowed roadway will include two 11 ft. travel lanes, and two 1.5 ft. shoulders. The path will narrow to 8 ft. wide with a 2 ft. planted landscape strip (Section A-A'). Following the right of way reallocation, the path will widen to 12 ft. with a 2.5 ft landscape strip. Two turn-up curbs and one turn-down curb are required within this segment. Temporary construction easements are required within this segment, as well as utility relocations.

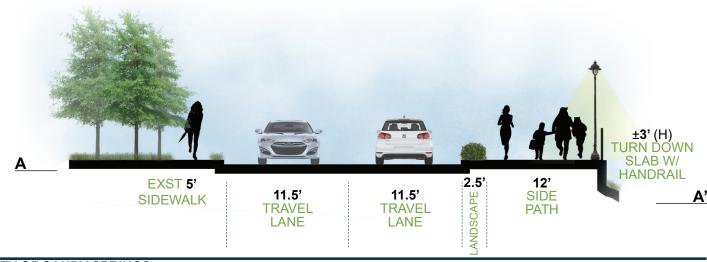


07.6 PHASE 2 ALTERNATIVE DESIGN (STA 61+00 TO 53+10)

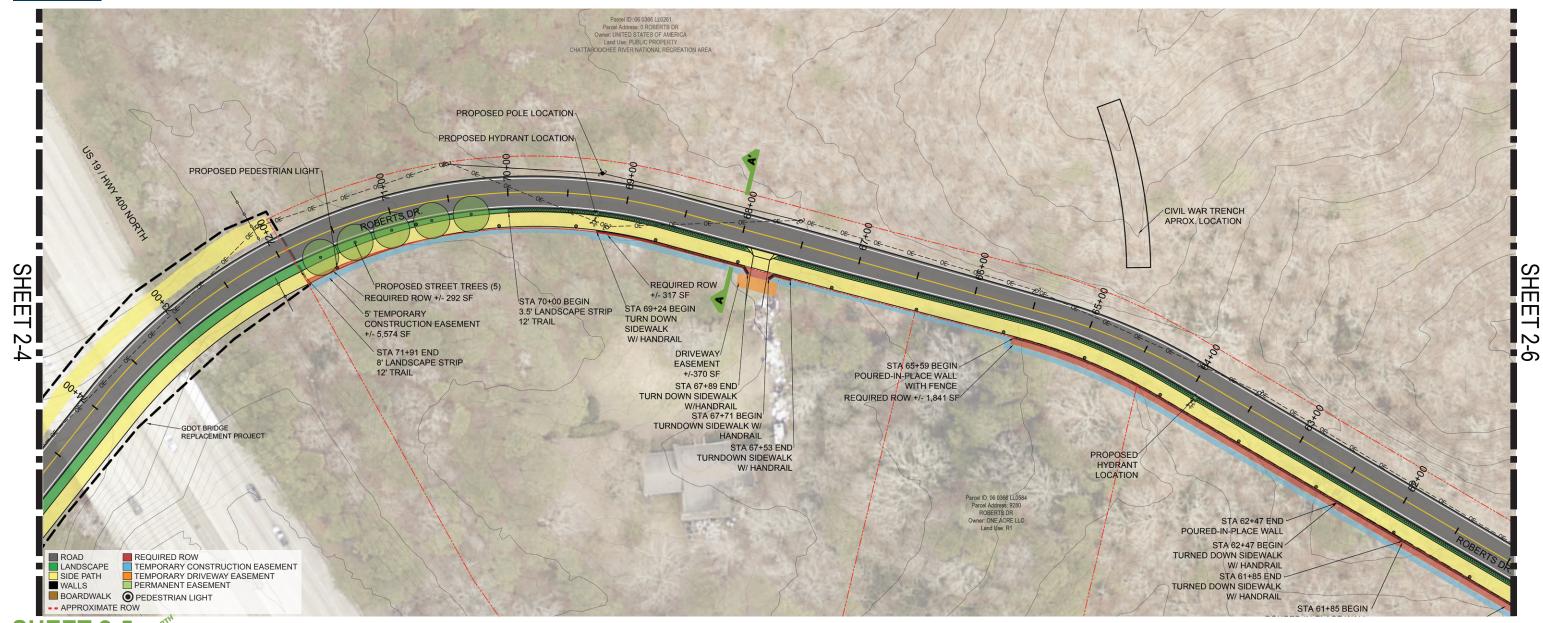




This sheet shows path alignment Phase 2 Alternative, where the side path remains continuous on the north side of Roberts Drive. This segment continues at STA 61+00 and ends at station 53+10, the end of Phase 2 where it will tie into Phase 1. The path continues at 12 ft. wide with a 2.5 ft. landscape strip. At STA 55+00, the landscape strip widens to 3.5 ft. One turn-down curb is required. Temporary construction easements are required within this segment, as well as utility relocations. Section A-A' shows the side path along the turn-down slab, with handrail.



SHEET 2-5 ALTERNATIVE DESIGN ALONG PARCEL 3900 ROBERTS DRIVE

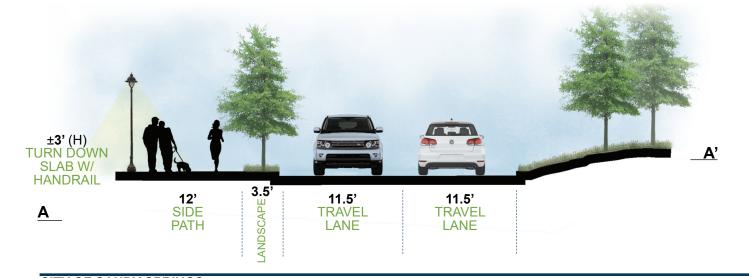


SHEET 2-5 NOT TO SCALE

This segment of the Phase 2 exhibits the sidepath design without the proposed development on parcel address 9300 Roberts Drive. The side path design along this property shown on sheet 2-5 (page 62-63), will be constructed by the property owner. In the event that the parcel is not developed, the side path will be designed as shown on this sheet. Below is a cost summary of the side path as shown here along the parcel 9300 Roberts Drive. If the property is not developed as planned, then the following cost will be added to the Phase 2 cost shown in the report (page 88).

Estimated Cost of side path along 9300 Roberts Drive, as shown on this sheet:

Total	\$ 367,288.00
O&P, Project management, contingency, design/eng. service	72,643.00
Turn-down curb with railing (9 cy)	40,950.00
Decorative Light Poles (6)	72,000.00
Side path(12' wide, 520 sy) + Agg. Base (520 sy)	67,600.00
Trees (5) + Sod (107 sy) + Shrubs (140) + Mulch (172 sy) + Topsoil (15 cy)	18,895.00
Required ROW (391 sf), 5 ft TCE (3026 sf)	95,200.00



ROBERTS DRIVE SIDE PATH SCOPING REPORT

07.8 RESOURCES

ARC GIS mapping, ESRI

City of Sandy Springs' Transportation Master Plan 2021, Kimley Horn 2021

City of Sandy Springs' Next Ten Comprehensive Plan 5-yr Update, adopted 2017, updated 2022, RHI (Rhodeside Harwell), Nelson / Nygaard, Code Studio, Gensler, RCLCO, Lowe Engineers, Ross & Associates

City of Sandy Spring' Recreation and Parks System Comprehensive Plan, February 2019, Barge Design Solutions, PROS Consulting and ETC Institute

City of Sandy Springs' Trail Master Plan, adopted Oct. 2019, PATH Foundation.org and KAI-ZEN Collaborative

City of Sandy Springs' TSPLOST 2021, Fulton County, GA

City of Sandy Springs' Bicycle, Pedestrian, and Trail Implementation Plan, December 2014, HDR Engineering, Inc.

City of Sandy Springs Zoning Ordinance Streetscape Manual Urban Design Standards, 2013, ORD NO. 2005-12-19

Construction Standards and Details, 2023 Georgia Department of Transportation (GDOT)

Preliminary Alternatives Assessment, Improving Bicycle and Pedestrian Connectivity to the Island Ford Unit of the Chattahoochee River National Recreation Area (Hagen Thames Hammons, 2015-16, NPF Transportation Scholar)

Small Town and Rural Multimodal Networks Guide, Dec. 2016, Alta Planning + Design (Alta), Small Urban and Rural Livability Center – Western Transportation Institute, and National Association of Counties, 711 SE Grand Avenue, Portland, OR 97214, U.S. Department of Transportation, Office of Planning, Environment, and Realty, Federal Highway Administration, 1200 New Jersey Avenue SE Washington DC 20590 FHWA-HEP-17-024

Atlanta Regional Commission (ARC) Metropolitan River Protection Act (MRPA) website: https://atlantaregional.org/what-we-do/natural-resources/metropolitan-river-protection-act/

THIS PAGE LEFT INTENTIONALLY BLANK.

ROBERTS DRIVE SIDE PATH SCOPING REPORT

